

How Safe are the Tourist Places in Delhi?

Safety Analysis Report

Tourist Places of Delhi

Safety Analysis Report

Executive Summary

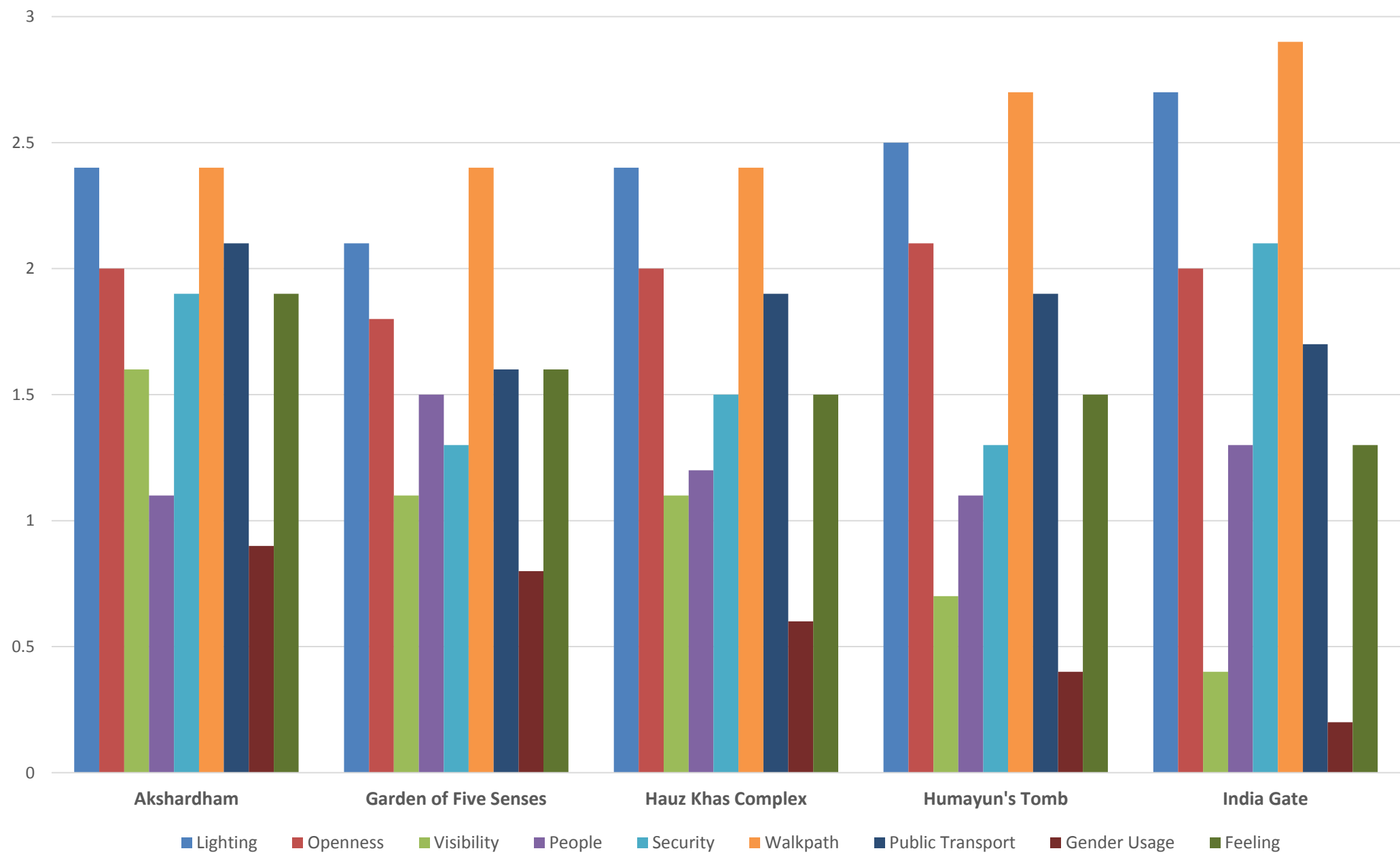


“How safe are the tourist places in Delhi?” is a project initiated by **SafetiPin** carried out in collaboration with **Delhi Tourism and Transportation Development Corporation (DTTDC)**. This report is based on the safety audits done in June – July 2016, around 10 most popular tourist destinations of Delhi. These places were selected in consultation with Delhi’s Tourism Department, on the basis of their footfall. Audits were conducted to analyze the condition of available infrastructure around them, and suggest recommendations on making tourist places safer, keeping in mind the large number of tourists that visit daily.

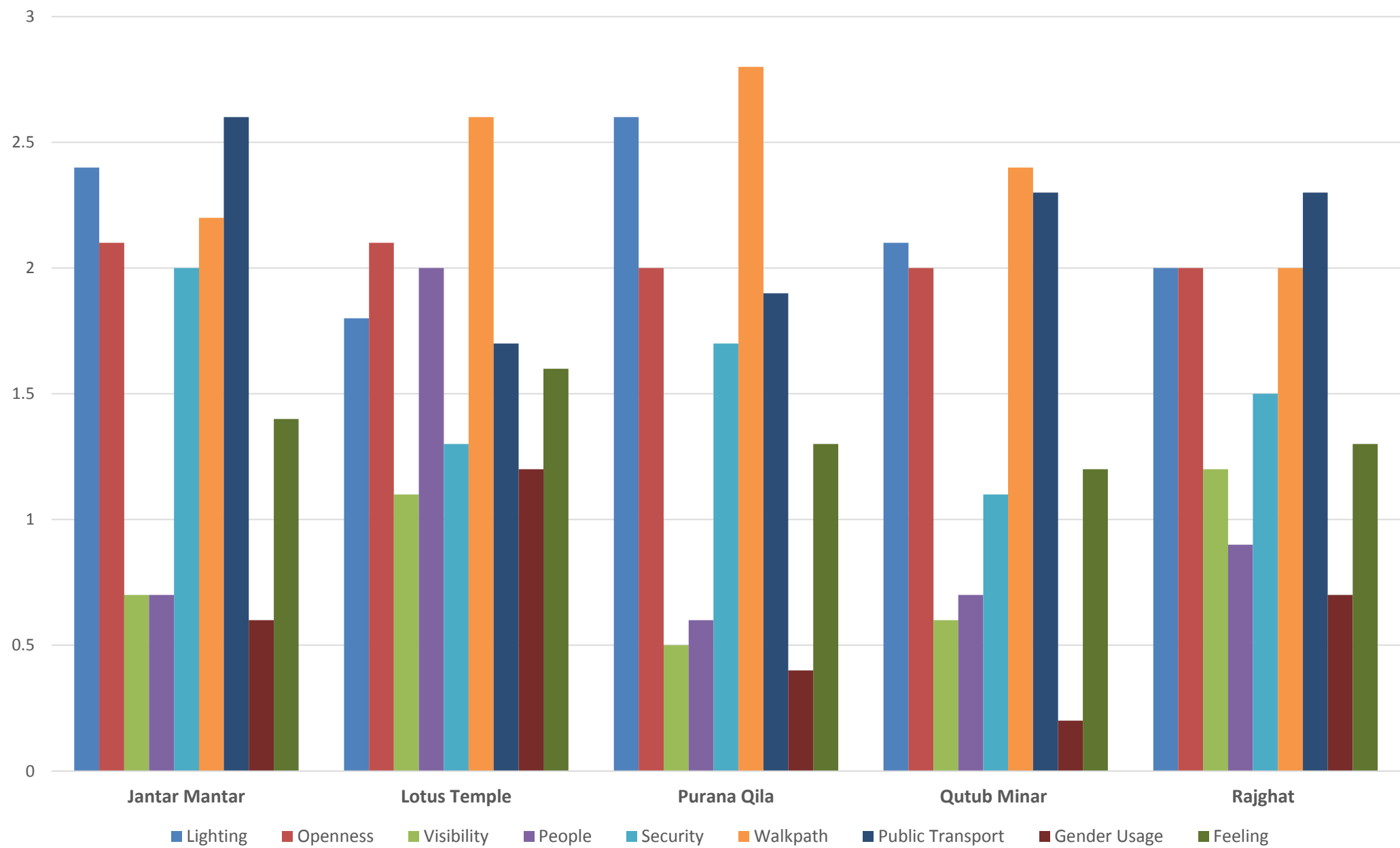
The audits were carried out using the “*My SafetiPin*” App. SafetiPin, is a map-based mobile phone and online application, which works to make communities and cities safer by providing safety-related information collected by users and by trained auditors. At the core of the app is the Safety Audit. A Safety Audit is a participatory tool for collecting and assessing information about perceptions of urban safety in public spaces. The audit is based on nine parameters – Lighting, Openness, Visibility, Crowd, Security, Walkpath, Availability of Public Transport, Gender Diversity and Feeling at a point. Results of the safety audit are aggregated to a safety score.

The access route to the tourist place i.e. from the nearest metro station to the entry gate of the tourist place was audited to assess the safety of pedestrians. The audits were done in the evening hours of 5pm - 9pm for the 10 tourist places, to evaluate the level of safety and identify ways to improve it. The ten tourist places selected for this project are Akshardham, Garden of Five Senses, Hauz Khas Complex, Humayun’s Tomb, India Gate, Jantar Mantar, Lotus Temple, Purana Qila, Qutub Minar and Rajghat. The average parameter rating for the tourist places can be seen on the next page.

Findings: Average Parameter Rating (out of 3)

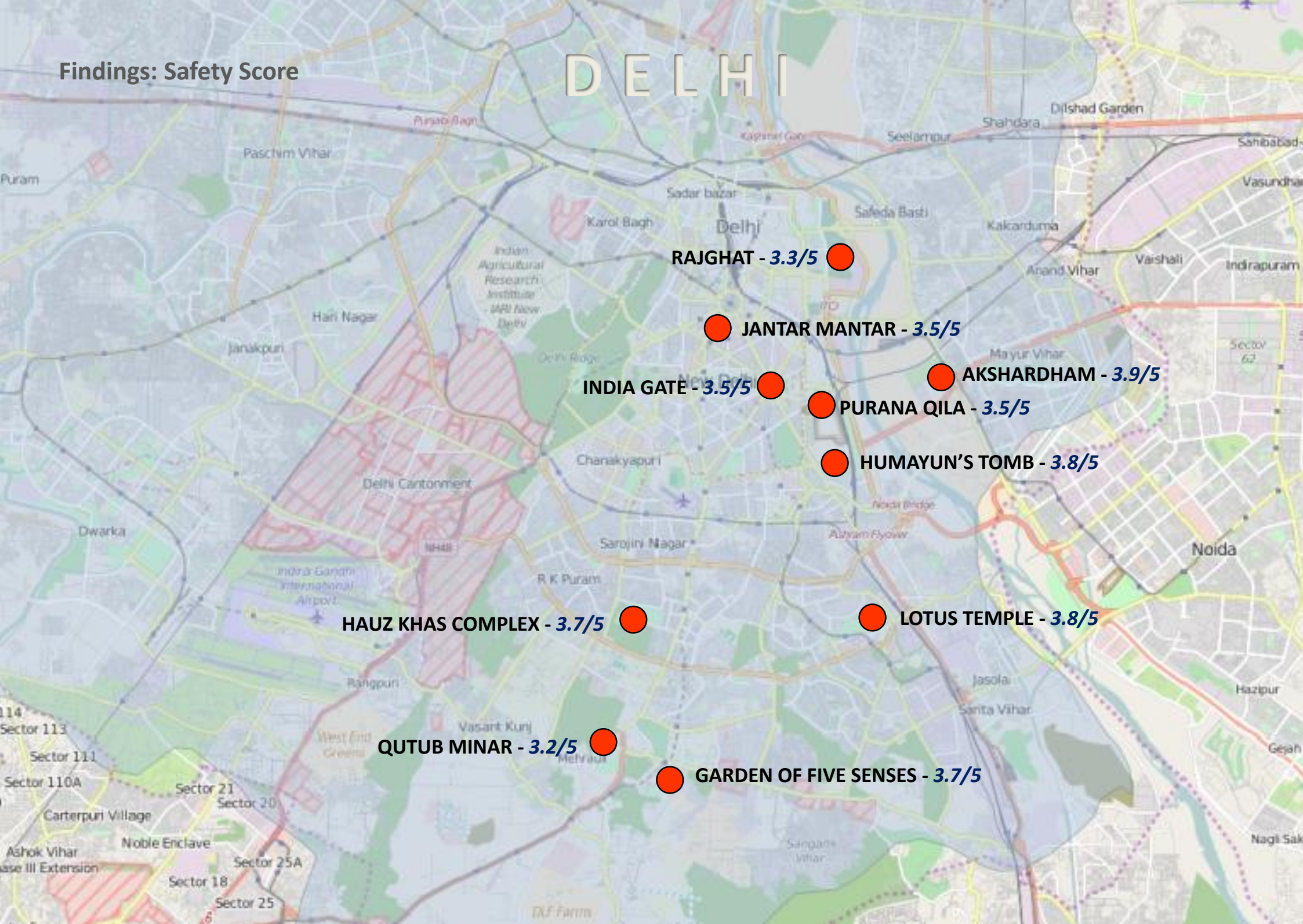


Findings: Average Parameter Rating (out of 3)



Findings: Safety Score

DELHI



Findings

▪ **Absence of Para Transit stand**

Public Transport has been rated above average at most of the places. Metro Station is present within 2 km distance, and bus stops are present at regular distance. Autos are available at metro station and the tourist place. But, in absence of proper parking space, they end up crowding the area around the entry/exit of the metro station and the tourist place.

▪ **Inaccessible footpath**

Walkpath have been rated Above Average as properly paved footpath exists at most of the places. However in some areas, they are occupied by street vendors and temporary stalls, forcing people to walk on the vehicular carriage-way. At some places, they were found to be broken and non continuous. Also, seen was lack of tactile paving on the footpath.

▪ **Low visibility**

Visibility has been rated below average at most of the places. Street Vendors present outside entry/exit of the tourist places offer good visibility. Since, they do not have a designated space, they end up occupying the footpath.

▪ **Poorly lit footpath**

Lighting has been rated Above Average in most parts. Though the area around the tourist places are well lit, there is a need to focus on footpath. Streetlights installed only on central median of the road are inadequate for the pedestrians.

▪ **Low pedestrian count post sunset**

People and Gender Usage has been rated below average and poor respectively. As most of the places closes by sunset, the area gets secluded and inactive.

Recommendations

▪ **Strengthen Para-Transit facility:**

To improve the para – transit facility, Auto Stands need to be created where they can be parked, clear of the pedestrians' movement. In addition to the tourist place and the metro station, these auto stands can be created along the bus shelters.

Additionally, a bicycle-rental system can be introduced at tourist places which are more than 500 m away from metro station or bus stop. Bicycle stands should be set up at regular intervals i.e. metro station, bus stops.

▪ **User friendly Footpaths:**

The footpath leading to the tourist place should be accessible through a gradual ramp and should have tactile paving, clear of any obstruction. At places where the nearest bus stop/ metro station is within 500m distance, a continuous footpath with tactile paving should be provided.

Wherever a footpath is wide enough, a designated space should be made for the hawkers, clear of the pedestrian pathway.

▪ **Create designated Hawker zones:**

A designated hawker zone should be set up, clear of the footpath and equipped with Public Convenience and street furniture. Along with hawker zone, souvenir shops can be set up outside the tourist complex.

▪ **Enhance Lighting:**

Pedestrian scale streetlights should be installed along the built edge facing the footpath, and cycle tracks in some cases.

Also, additional lights should be installed inside the parking lot to ensure that the area is brightly lit at night. Currently, streetlights are installed only at the entry/ exit of a parking lot, resulting in a poorly lit space.

▪ **Promote Night Tourism:**

Night Tourism can be promoted to ensure activity around the tourist places, could contribute to the feeling of safety. This can be done through heritage walks, photo walks, food walks, garden festivals etc.

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INTRODUCTION



Location of tourist places

“How safe are the tourist places in Delhi?” is a project initiated by SafetiPin carried out in collaboration with Delhi Tourism and Transportation Development Corporation (DTTDC).

The project is based on safety analysis of tourists in Delhi using a mobile app, My SafetiPin. My SafetiPin is a map-based mobile phone application, that works to make communities and cities safer by providing safety-related information collected by trained auditors. At the core of the app is the Safety Audit. A Safety Audit is a participatory tool for collecting and assessing information about perceptions of urban safety in public spaces. The audit is based on nine parameters: Lighting, Openness, Visibility, Crowd, Security, Walk path, Availability of Public Transport, Gender Usage of Public Spaces and Feeling.

The project was carried out by conducting manual audits i.e. observing the late hour activities on the road and local streets, pedestrian count and the physical infrastructure available for pedestrians. All these manual audits were done on foot from the nearest metro station to the tourist spot at every 20m. A total of 10 tourist spots in Delhi were selected, in consultation with Delhi's Tourism Department. The audits were done in the evening from 5 PM - 9 PM for all the tourist places and a total of 619 audit pins were generated at the ten tourist places.

Along with the physical image and structure, the social image of an area plays an immensely important role towards the perception of safety and more importantly crime rate in that area. In other words, the social image of an area also, to an extent, affects the rate of crimes in an area.

This project will help in identifying the aspects to be worked upon, in order to ensure safety in and around the tourist places, and also in laying down standards for other areas.

About

Akshardham Temple is a Hindu temple located in East Delhi near Noida Mor. Officially opened on 6 November 2005 by Dr. A.P.J. Abdul Kalam. the temple, which attracts approximately 70 percent of all tourists who visit Delhi. It is situated near the banks of the Yamuna adjacent to the 2010 Commonwealth Games village in eastern part of the capital.

Social Usage

The area only has a temple complex and residential quarters for temple authorities. While it is active during the day, the area seems secluded after the people retire into their quarters. During the day, the stretch between Akshardham metro station and the temple has numerous road side vendors occupying the footpath. Constant police surveillance is seen throughout the day. In addition to that, the temple has its own security force.

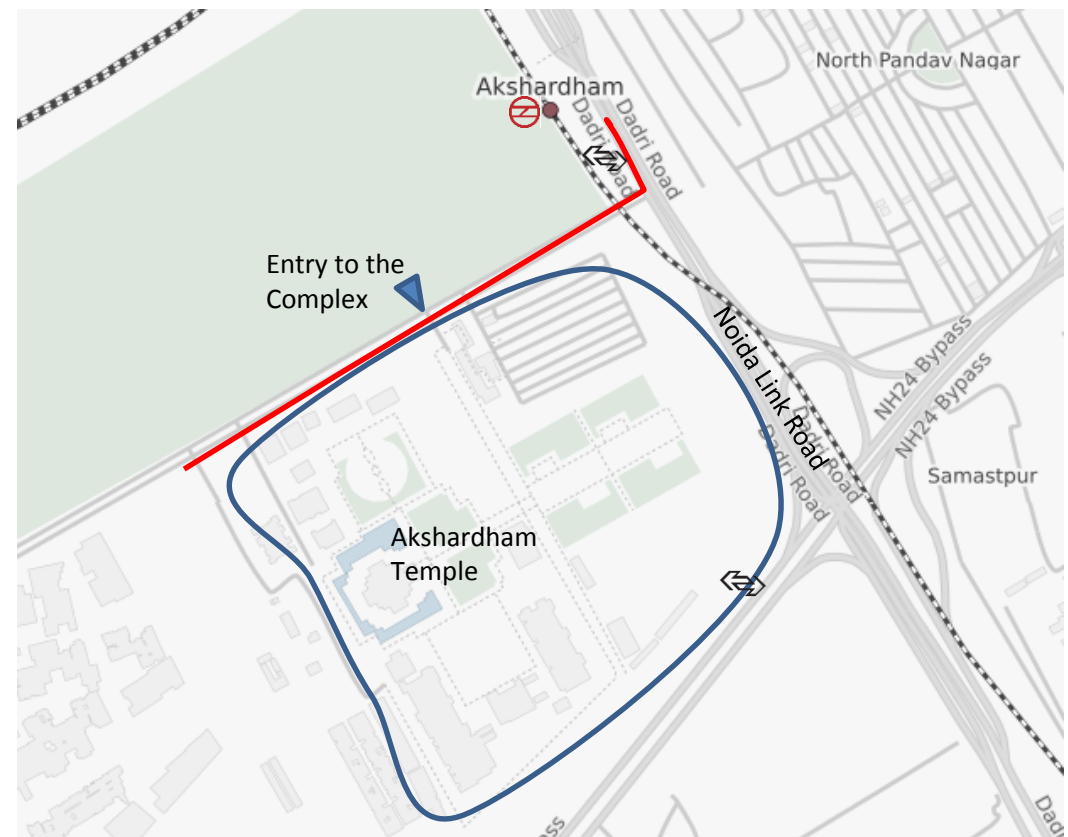


Akshardham Temple

Safety Audit

The route from Akshardham metro station to Akshardham Temple i.e. approximately 500m has been audited and 29 audit pins have been generated. The nearest bus stop is located 400m away from the temple.

Nearest Metro Station – Akshardham (Blue Line)












Location of the tourist spot

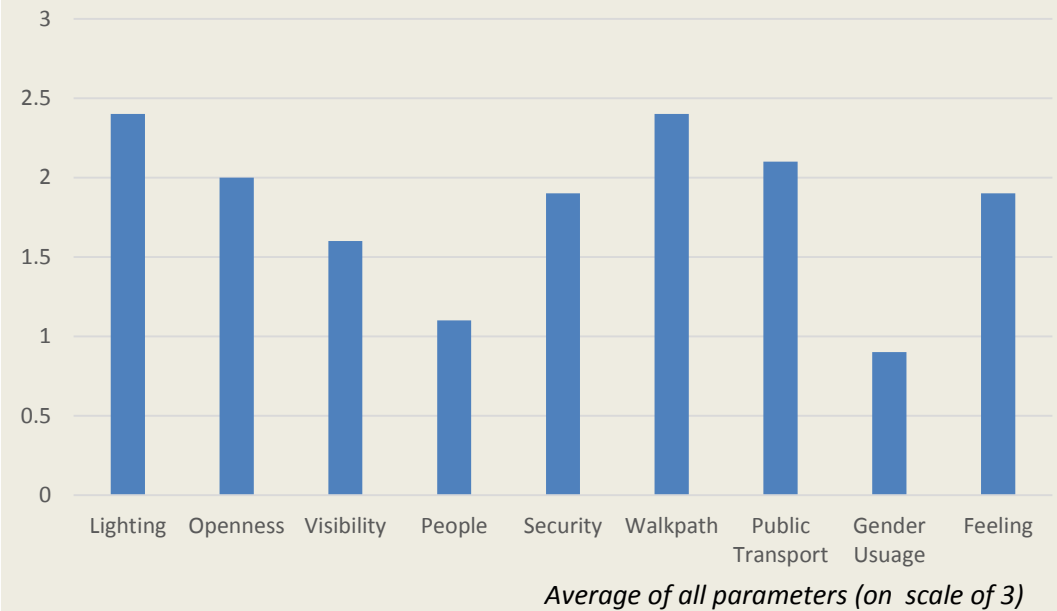
⊘ Metro Station

↔ Bus Stop

— Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	2.4 Above Average The road to the temple and Delhi-Noida Link Road has adequate lighting.
 Openness	2.0 Above Average The road is a six lane wide with Akshardham temple on one side and residential colonies on the other.
 Visibility	1.6 Average Visibility parameter is average due to the presence of vendors on route from metro station to the temple.
 People	1.1 Below Average The pedestrian count decreases towards the night.
 Security	1.9 Above Average Police Patrolling Van is present near the temple and the temple has its own private security..
 Walk Path	2.4 Above Average Footpath exists but occupied by roadside vendors on route from metro station to the temple.
 Public Transport	2.1 Above Average The nearest access to public transport is the metro station and bus stop i.e. 500m away from the tourist spot. No designated auto stand throughout the stretch.
 Gender Usage	0.9 Below Average Gender diversity decreases towards the night.
 Feeling	1.9 Above Average Overall, auditors rated Feeling as Above Average.



The path leading to the temple

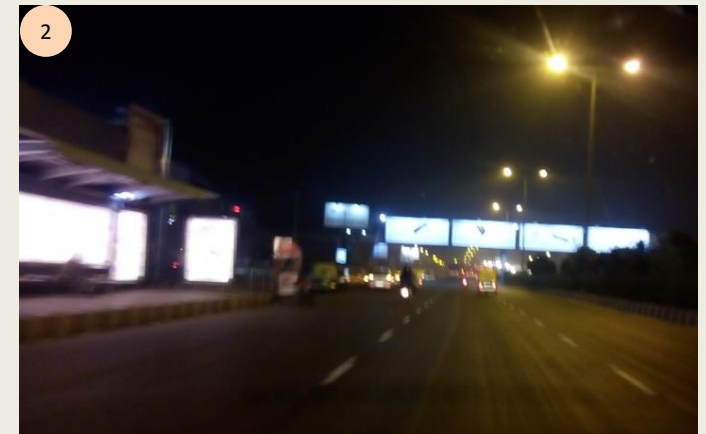
SAFETY SCORE



Map showing Safety Score rating



Safety Score is rated excellent at this audit point, owing to high rating in the Lighting, Walkpath, Security and Feeling parameters.



Safety Score is rated poor at this audit point, owing to low rating in the Visibility, Security and Feeling parameters.

VISIBILITY

Visibility is rated 1.6/3 i.e. Average. Street vendors and temporary stalls are present near the metro station on Noida Link Road. However, they are active only till 8 PM., post which the audit stretch offers low visibility.



- 1 On the stretch from the metro station to the Akshardham Temple, various
- 2 temporary stalls and street vendors are present. In absence of any designated space, they end up obstructing pedestrian's movement on the footpath (image 2). A hawker zone should be created along the built edge, clear of the footpath.
- 3 With no street vendors (image 3) and
- 4 high boundary walls (image 4), this road to the temple offers poor visibility. As seen in image 3, the footpath on the left side is wide enough for setting up a hawker zone. A hawker zone equipped with street furniture should be set up here. Also, the solid surface of the boundary wall on the right side needs to be reduced to atleast 1m and the remaining height can be achieved using a grill.



GARDEN OF FIVE SENSES

Safety Score: **3.7** / 5

About

The Garden of Five Senses is a park spread over 20 acres, located near Saket metro station (Yellow line). Inaugurated in February 2003, the garden is designed to stimulate our five senses with its beauty and give us a chance to touch, smell, hear and see our natural surroundings. The garden serves as one of the prominent cultural venues of Delhi, as programmes are organized here round the year. Garden tourism festival (February), food festivals, Dandiya festival are some of the cultural programmes held here.

Social Usage

The walk path near the garden is used by residents of nearby areas for a peaceful nocturnal walk in a calm environment. This area is frequented by youth as it has various food joints and cafes. In addition to that, tuition centres and offices are also located here. They contribute evenly in the pedestrian count as seen on the road. Police check post present near the park is only operational during the night.

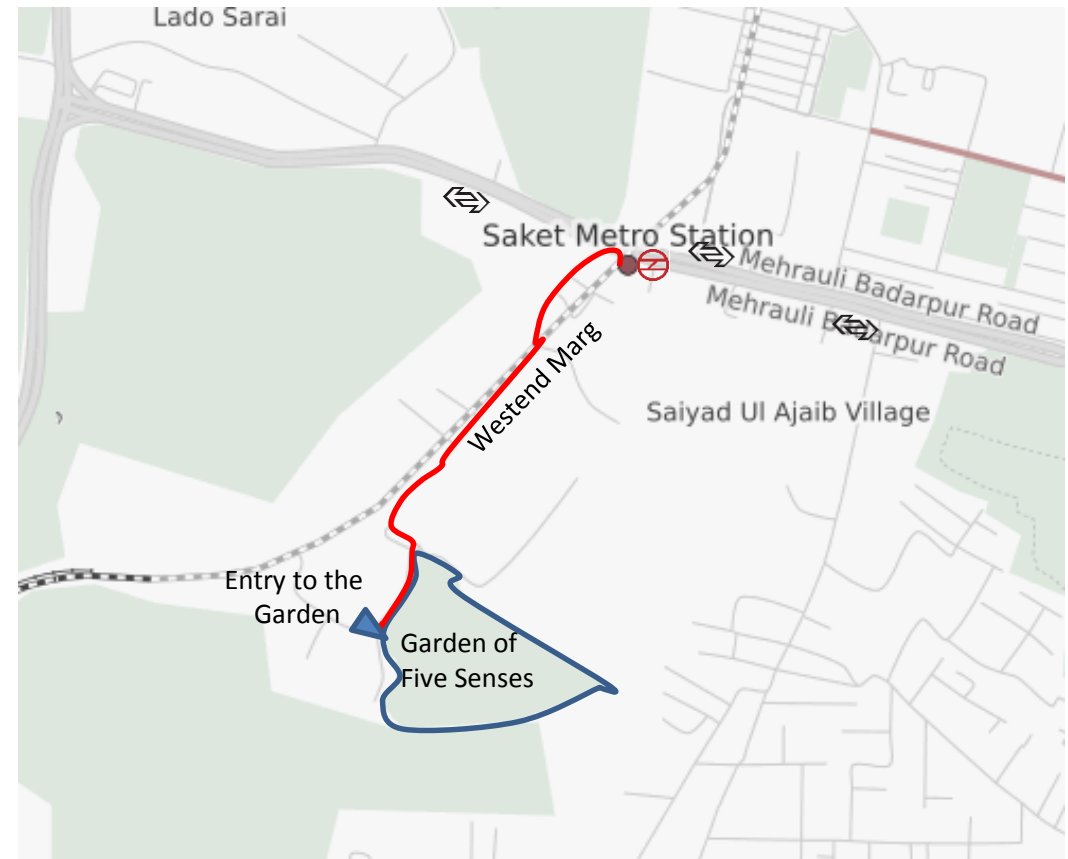


Garden of Five Senses

Safety Audit

The route from Saket metro station to the Garden of Five Senses i.e. approximately 900m has been audited and 47 audit pins have been generated. The nearest bus stop is 1.1 km away from the garden. The area outside the metro station entry/exit and the commercial area at the Westend Marg were studied.

Nearest Metro Station – Saket (Yellow Line)












Location of the tourist spot

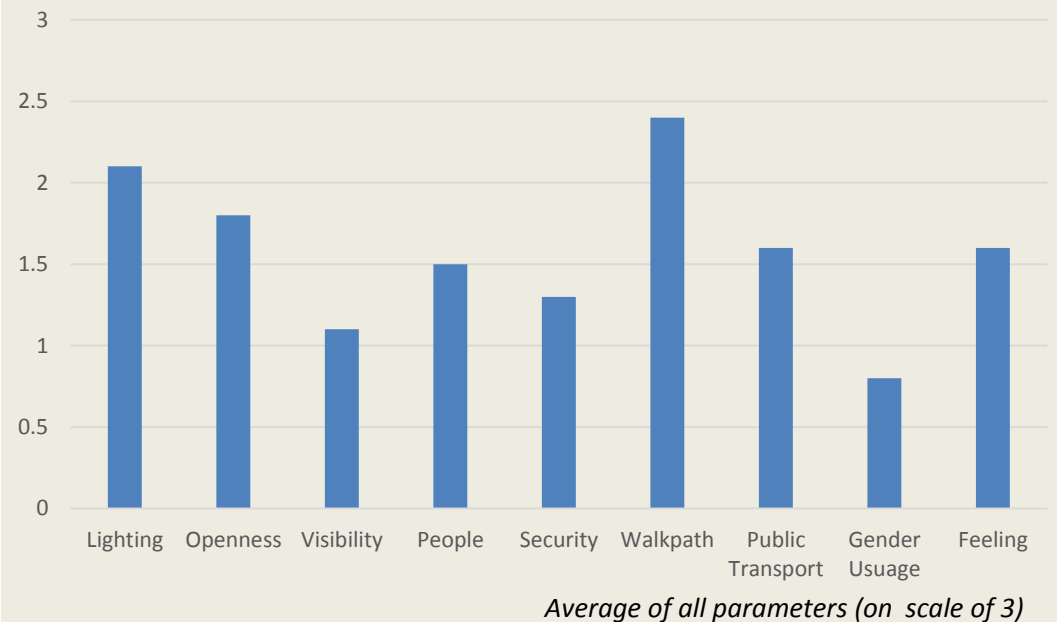
⊘ Metro Station

⇌ Bus Stop

— Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	2.1 Above Average Lighting is rated above average, however street lights are placed only on one side of the road.
 Openness	1.8 Average The road is a three lane wide with an open ground in the east at a slight elevation, currently used for car parking.
 Visibility	1.1 Below Average Visibility parameter is low due to “less eyes on the street” on the route from the metro station to the market, and then from the market to the garden.
 People	1.5 Average The pedestrian count decreases towards the night, only students and local residents are seen.
 Security	1.3 Average A Police check point is present in the market on Westend Marg.
 Walk Path	2.4 Above Average Footpath exists but obstructed by street vendors and shops’ banners at most of the spots in the market.
 Public Transport	1.6 Average The nearest access to public transport is the metro station i.e. 900 m away from the tourist spot. No designated auto stand throughout the stretch.
 Gender Usage	0.8 Below Average Gender diversity decreases towards the night.
 Feeling	1.6 Average Overall, auditors rated Feeling as Average. At night with high boundary walls around and no activity, pedestrians don’t feel comfortable.



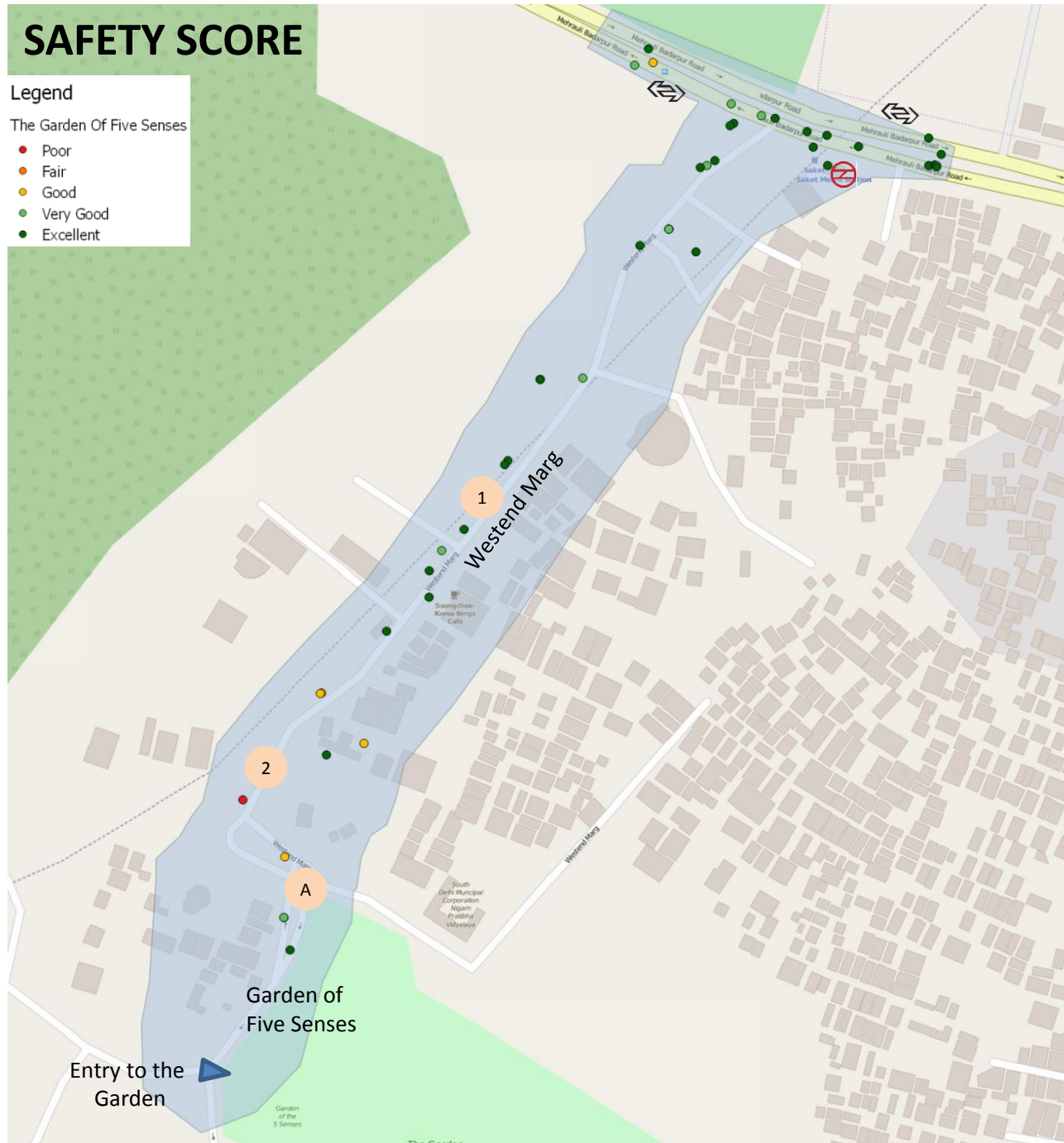
The path leading to the garden

SAFETY SCORE

Legend

The Garden Of Five Senses

- Poor
- Fair
- Good
- Very Good
- Excellent



Map showing Safety Score rating



Safety Score is rated excellent at this point, owing to high rating in the Visibility, People, Security, Gender Usage and Feeling parameters.



Safety Score is rated poor at this point, owing to low rating in the Visibility, People, Security, Public Transport, Gender Usage and Feeling parameters.

LIGHTING

Lighting is rated 2.1/3 i.e. Above Average. Street lights are present only on one side of the road. As a result, few dark spots were observed on the other side of the road. Also, lighting in parking spaces near the garden and the market was found to be inadequate.

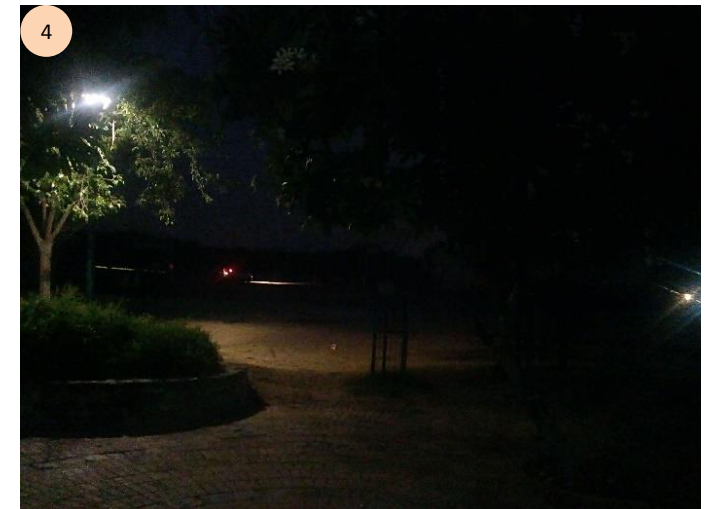


1 On the stretch from the metro station to the market on the Westend Marg, light poles are installed only one side of the road. Additional light poles should be installed alternately on the other side of the road, that will ensure uniform illumination throughout the road.

2 Some lights were found to be non-operational at the time of audits. They need to be checked and made operational.

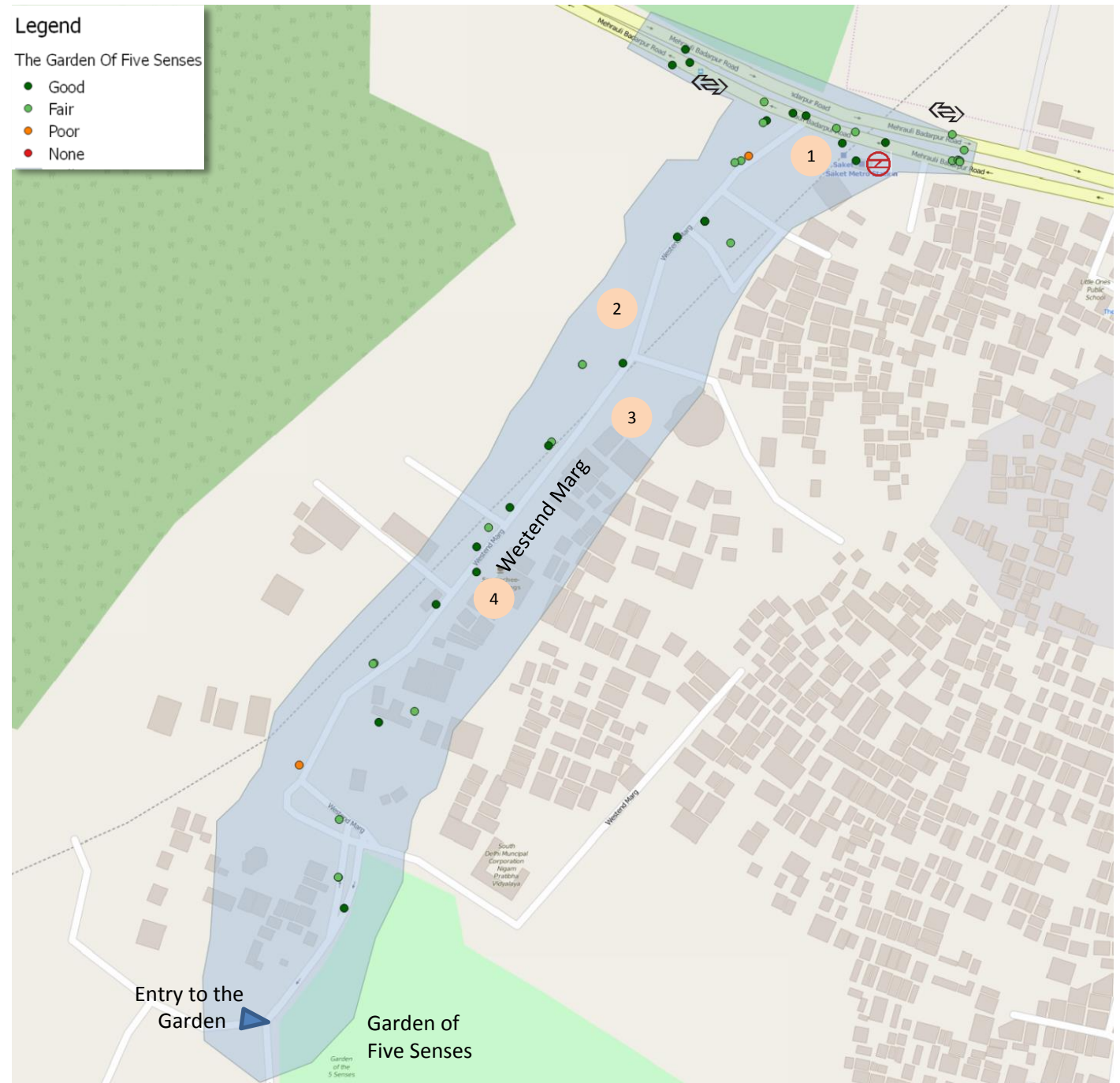
3 The parking space provided for tourist buses is not well lit. There is only one light installed at the entry. As a result rest of the area becomes completely dark at night. Additional lights should be installed to avoid dark spots.

4 Similarly, additional lights should be installed in the parking area provided for private vehicles. Also, the foliage along the streetlight needs to be pruned regularly as the overgrown leaves shield the light fixture resulting in poor illumination.



WALK PATH

Walk path is rated 2.4/3 i.e. Above Average. Footpath exists in most of the audit path but is unusable at many spots. Along the Westend Marg, footpath is obstructed by vehicles, shops and roadside vendors, thus forcing pedestrians to walk on the road. A designated space should be created for the street vendors and temporary stalls, clear of the footpath. Also, vehicular parking on the footpath should be disallowed.



1 The footpath doesn't exist in some parts of the audit stretch, that forces pedestrians to walk on the road. In case of narrow road as seen in the image, a proper paved footpath should be constructed on one side, clear of any obstruction.



2 The footpath is completely blocked by Public Convenience and parked cars. The footpath should be built around the toilet to provide proper access. Parking on footpath should not be disallowed.



3 Footpath should be widened wherever it is obstructed by a tree. Also, vehicular parking should be disallowed on the footpath.

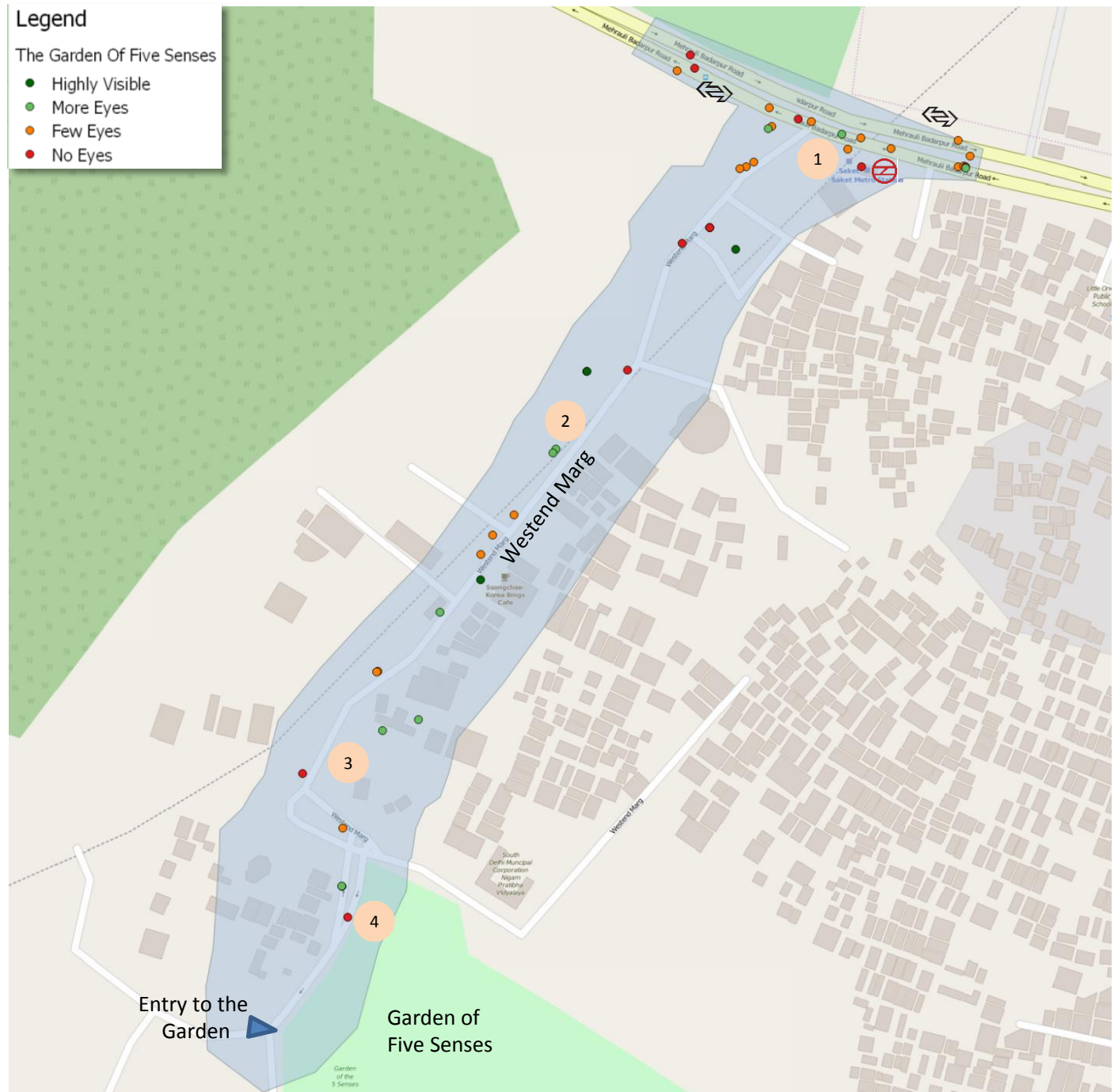


4 Shops and street vendors have occupied the footpath as seen in the image. The banners and other obstructions present in the front of the shops should be removed. At places where footpath is wide enough, a segregated space should be provided for street vendors in a way that doesn't obstruct the pedestrian movement.



VISIBILITY

Visibility is rated 1.1/3 i.e. Below Average. This is due to absence of “eyes-on-the-street” in most parts of the audit path. The market and commercial area are the only places with good visibility. In other areas especially between the market and the garden, one feels unsafe with high boundary walls and no activity around.



1 *Street Vendors present outside entry/exit of the Metro Stations offers good visibility. However, there is no designated space for them. A designated hawker zone should be set up, equipped with street furniture and public convenience.*



2 *The footpath leading to the market is largely inactive but occupied by street vendors at some distance. Public convenience facility is present but not operational. This space can be further enhanced by making it a hawker zone, clear of the footpath.*



3 *The route from the market to the garden is flanked by high boundary walls as seen in image. The solid surface of the boundary wall needs to be reduced to at least 1m and the remaining height can be achieved using a grill. Also, a footpath should be constructed along one side on this stretch.*



HAUZ KHAS COMPLEX

Safety Score: **3.7** / 5

About

Hauz Khas Complex is located in South Delhi. The complex consists of a lake or the royal water tank with several structures i.e. a mosque, a madrasa, a tomb overlooking the lake. The stretch from Aurobindo Marg leading to the complex has many small structures dating back to 13th century.

Social Usage

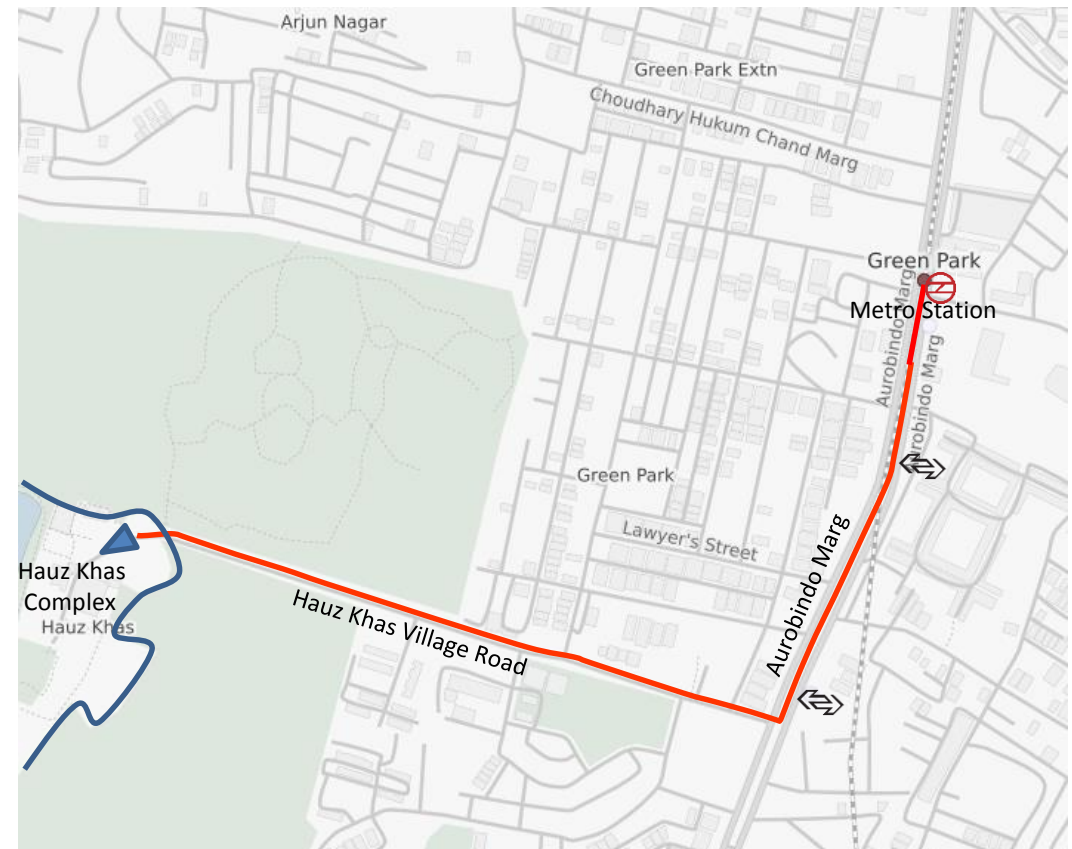
This complex offers a blend of ancient and contemporary environments. Adjoining the complex is Hauz Khas Village which has numerous cafes, restaurants and clubs. This urbanized village is now a relatively expensive tourist cum commercial area. Frequent by youth, Hauz Khas Village is active till late night. Police Patrolling Van is present along Aurobindo Marg.



Safety Audit

The route from Green Park metro station to Hauz Khas Village i.e. approximately 1.7 km has been audited and 99 audit pins have been generated. The nearest bus stop is located at a distance of 950 m from the Hauz Khas Complex. The route from the metro station on Aurobindo Marg to Hauz Khas Village was studied.

Nearest Metro Station – Green Park (Yellow Line)












Location of the tourist spot

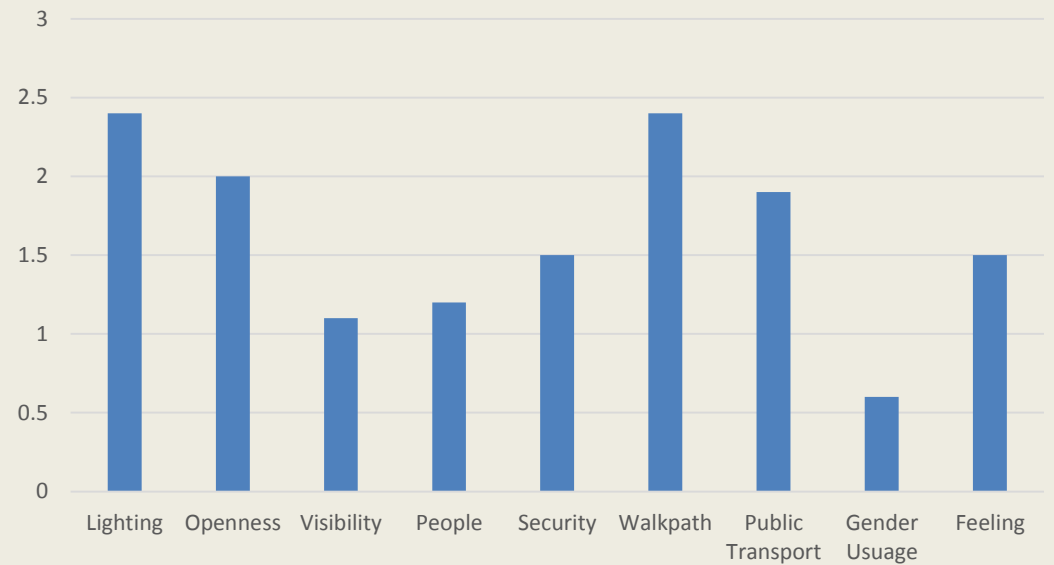
⊗ Metro Station

↔ Bus Stop

— Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	2.4 Above Average Street lights are provided throughout the audit stretch.
 Openness	2.0 Above Average The main road is wide and have a service lane. Hauz Khas Village road is a neighbourhood road with comfortable street scale.
 Visibility	1.1 Below Average Street Vendors are present near the Aurobindo Market and Hauz Khas Village. Rest of the stretch offers low visibility.
 People	1.2 Below Average Though Hauz Khas Village opens till late night, pedestrian count on the audit stretch decreases towards the night..
 Security	1.5 Average Police Check Post is present near Hauz Khas Village at night.
 Walk Path	2.4 Above Average Footpath exists but is non- continuous and broken at some places.
 Public Transport	1.9 Above Average Bus Stops are present at regular distance and autos are easily available.
 Gender Usage	0.6 Poor Gender Usage is rated poor as few women and children were seen during the time of audits
 Feeling	1.5 Average With low visibility on Hauz Khas Village road, pedestrians don't feel comfortable at night.

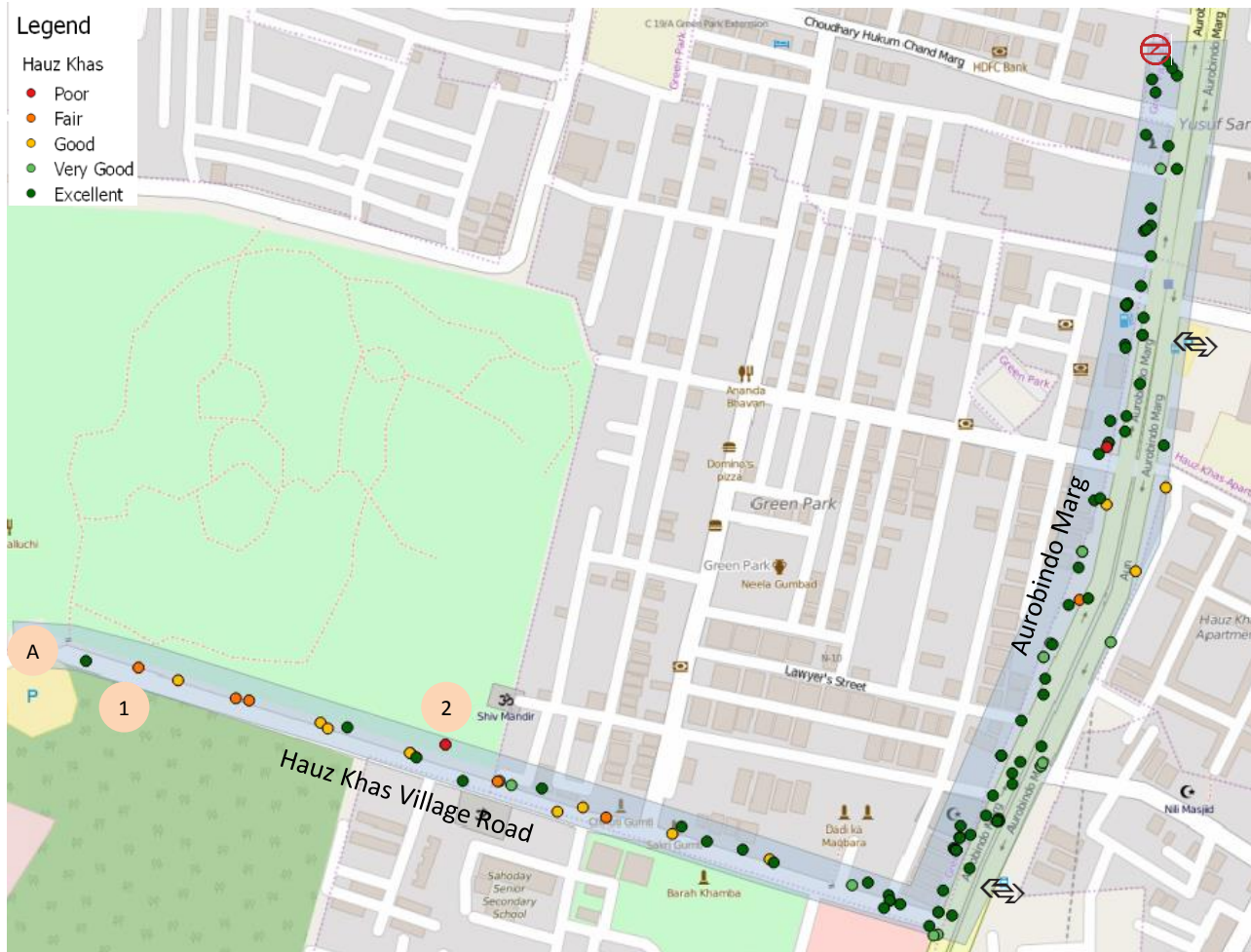


Average of all parameters (on scale of 3)



The path leading to Hauz Khas Complex

SAFETY SCORE



Map showing Safety Score rating



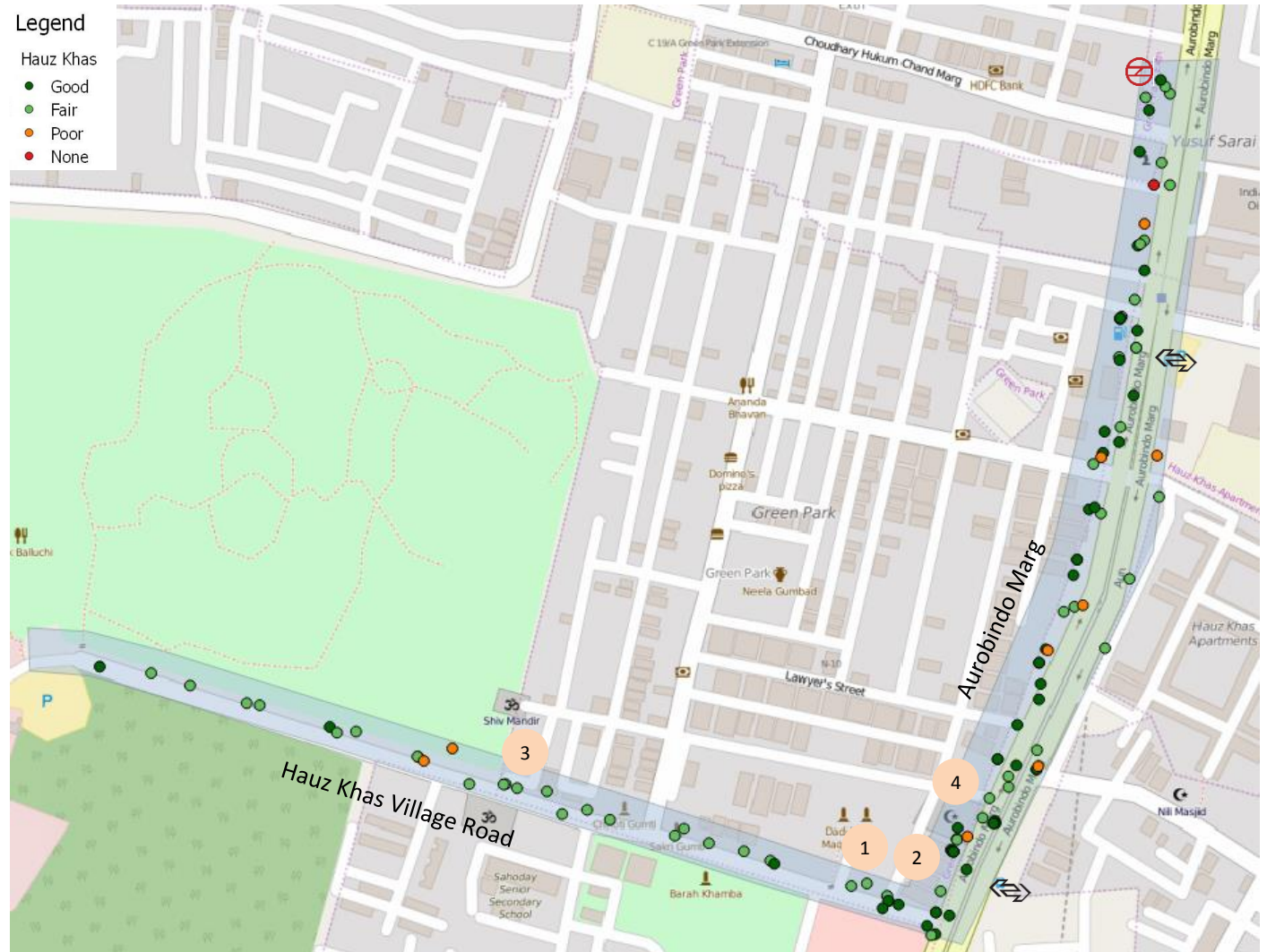
Safety Score is rated excellent at this point, owing to high rating in the Visibility, People, Gender Usage and Feeling parameters.



Safety Score is rated poor at this point, owing to low rating in the Visibility, People, Gender Usage and Feeling parameters.

WALK PATH

Walk path is rated 2.4/3 i.e. Above Average. However, existing walkpath on Hauz Khas Village road was not found to be user friendly. Also, the footpath on Aurobindo Marg is non- continuous and broken at some places. At some places, cars parked on the footpath obstructs the pedestrians' movement.



1 On the road to Hauz Khas Village, the footpath present is occupied by temporary stalls. They should be removed from here and shifted at the junction of Hauz Khas Village road and Aurobindo Marg (image 2) which has several other temporary stalls, clear of the walkpath.

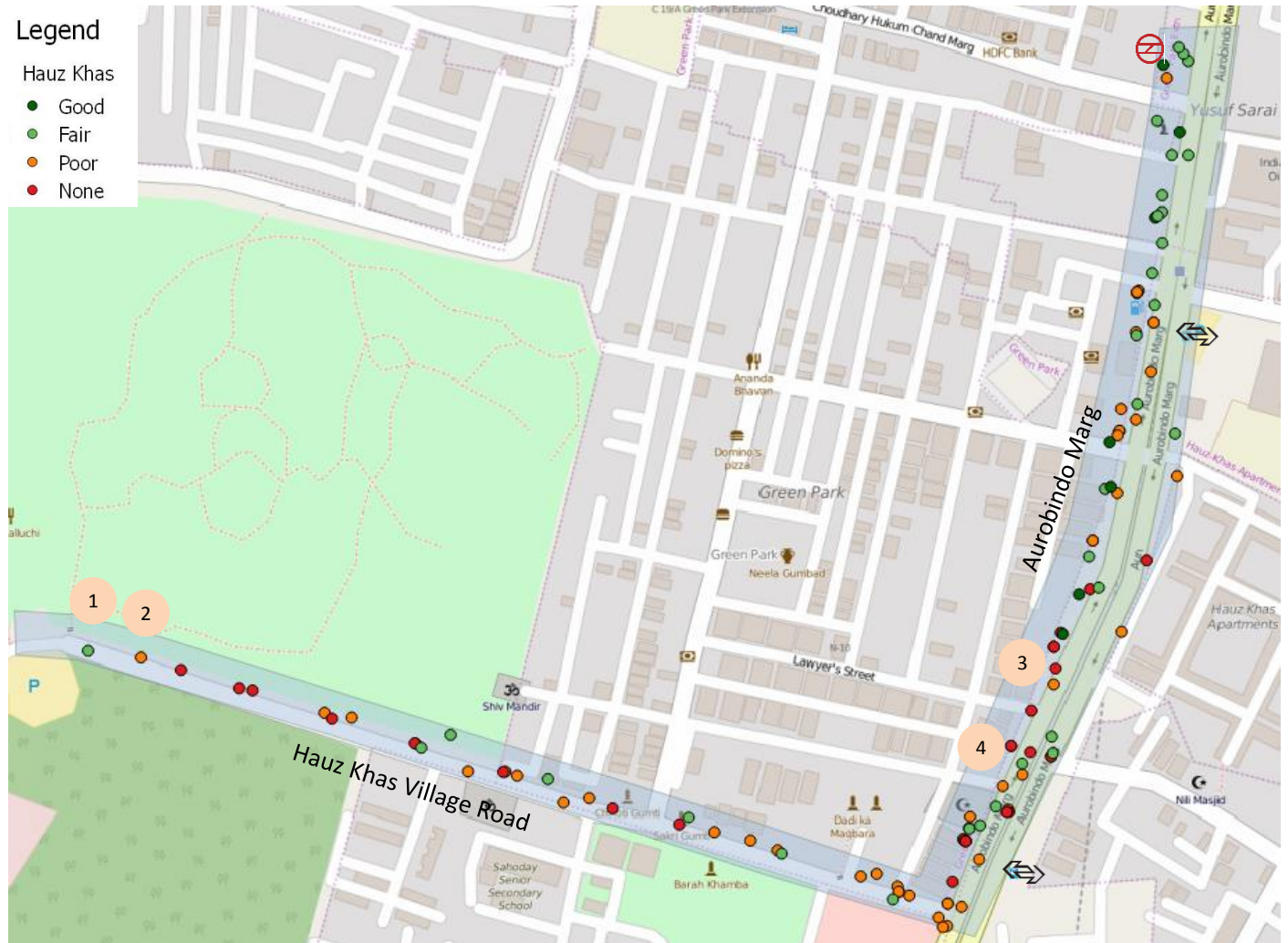
3 Further down this road, there is no footpath. This is a two lane road which experiences heavy traffic. Currently, the walkpath for pedestrians is flushed with the road, therefore obstructed by parked cars. A footpath should be constructed and parking should be restricted to other side of the road.

4 In front of the Mosque on Aurobindo Marg, the footpath is broken. Also, there is no level difference between the footpath and the service road. To provide a continuous and smooth movement to pedestrians, this footpath should be repaired and a kerb should be constructed to segregate it from the service road. Also, wherever footpath is obstructed by a tree, it should be widened.



VISIBILITY

Visibility is rated 1.1 i.e. Below Average. Road leading to Hauz Khas Village offers low visibility as the whole route is flanked by parked cars. As a result of buffer space between residences and the walkpath, there are no eyes on the street. The street vendors are present at entry/exit of the Hauz Khas Village, and in front of Aurobindo Place Market.



1 Near the entry of Hauz Khas Village, there are few hawkers present. A space should be designated for them by creating niches in the boundary wall of the park (image 2), clear of the walkpath. Also, a designated auto stand should be made where autos can be parked clear of the footpath and traffic.

3 Along the Aurobindo Marg, due to the planters between the service lane and the footpath, this stretch offers low visibility. To make this stretch active, this space should be redesigned to integrate it with the footpath. A shaded space equipped with street furniture should be provided for the pedestrians, which can accommodate the street vendors as well.



HUMAYUN'S TOMB

Safety Score: **3.8** / 5

About

Humayun's Tomb is located near Nizamuddin East. It was built in 1565 nine years after the death of Humayun by his widow, Bega Begam. The site was chosen on the banks of Yamuna river, due to its proximity to Nizamuddin Dargah, the mausoleum of the celebrated Sufi saint, Nizamuddin Auliya, who was much revered by the rulers of Delhi.

Social Usage

The Humayun Tomb Complex was declared a UNESCO World Heritage Site in 1993 and since then the place has gone under extensive restoration work by the government with Aga Khan Foundation. After the restoration work, the conditions in and around this complex underwent a sea-change. All the stalls and other intrusions were removed and the monuments and green spaces restored. A museum showcasing the restoration work is currently under construction.

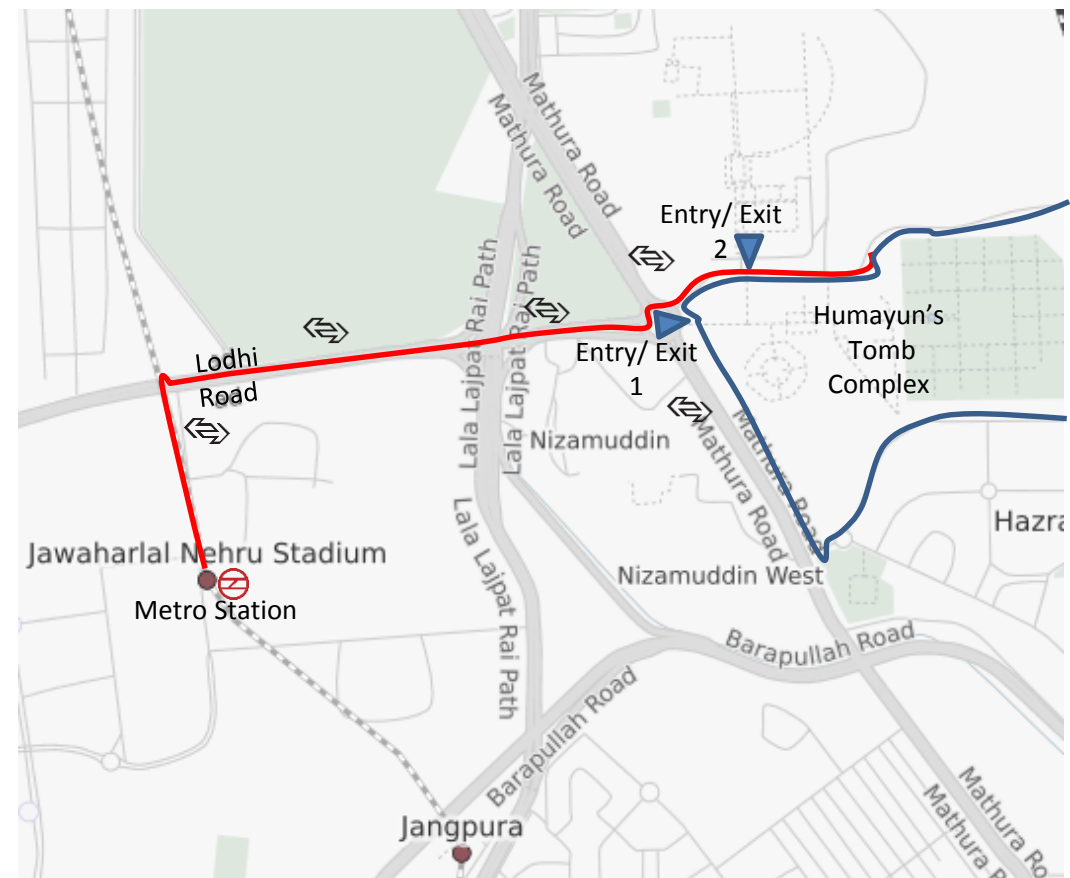


Humayun's Tomb

Safety Audit

The route from JLN Stadium metro station to the Humayun's Tomb i.e. 1.3 km has been audited and 90 audit pins have been generated. The nearest bus stop is 150 m away from Gate no. 1 of Humayun's Tomb Complex. The area outside the Humayun's Tomb Complex entry/exit and the stretch on Lodhi Road were studied.

Nearest Metro Station – JLN Marg (Violet Line)












Location of the tourist spot

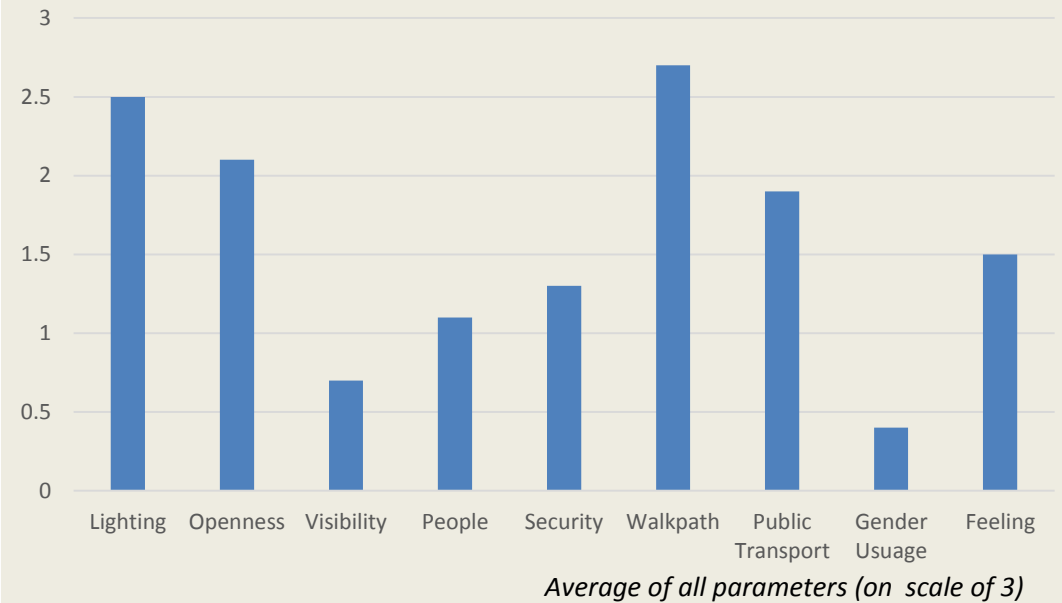
⊗ Metro Station

↔ Bus Stop

— Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	2.5 Good Street lights are provided along the central median as well as along the footpath.
 Openness	2.1 Above Average Four lane wide road flanked by cultural institutions and government offices.
 Visibility	0.7 Below Average High boundary walls and absence of hawkers on most of the audit stretch.
 People	1.1 Below Average People is rated below average since pedestrian count is low.
 Security	1.3 Average Some Police patrolling is done on the audit stretch.
 Walk Path	2.7 Good Walk path is present throughout the audit stretch.
 Public Transport	1.9 Above Average Bus Stops are present at regular distance and autos are easily available.
 Gender Usage	0.4 Poor Gender Usage is rated poor as few women and children were seen at the time of audits.
 Feeling	1.5 Average Due to low visibility on most of the audit stretch, feeling is rated average.



The path leading to Humayun's Tomb Complex

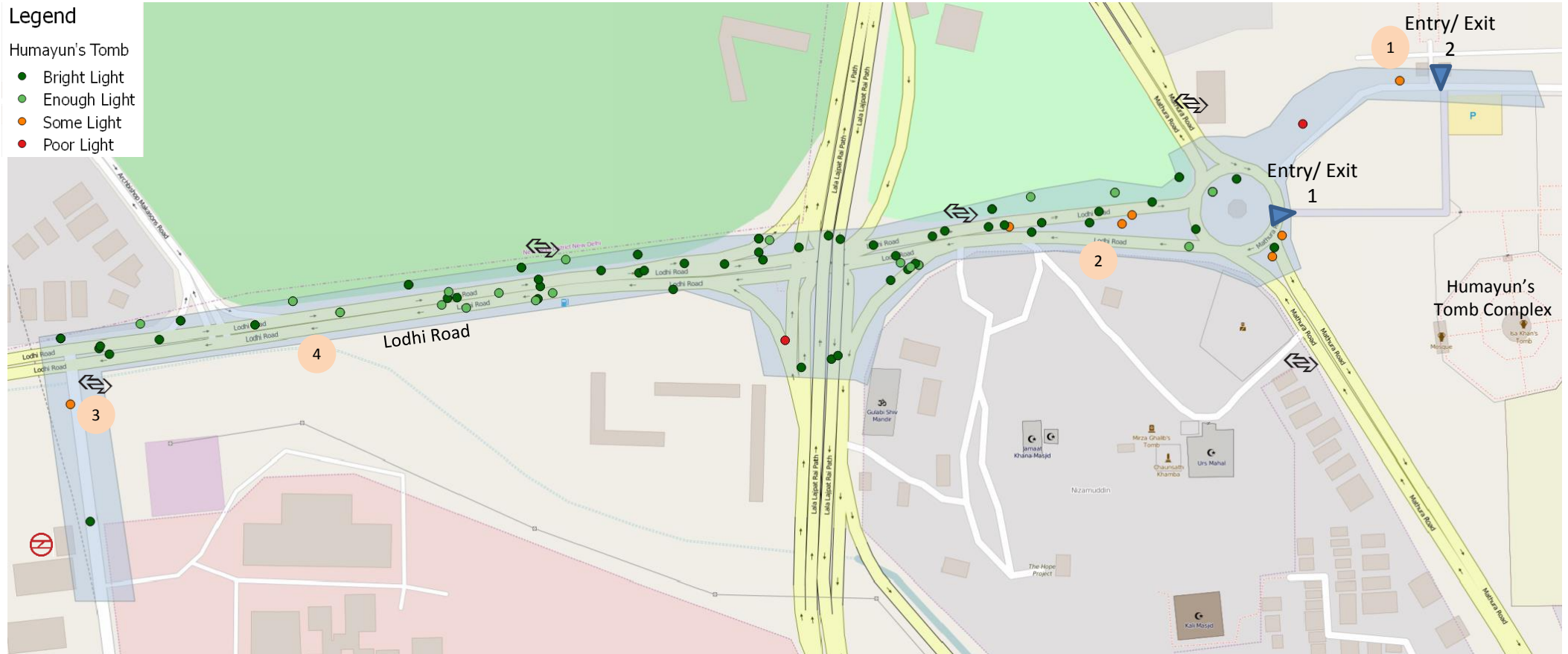
SAFETY SCORE



Map showing Safety Score rating

LIGHTING

Lighting is rated 2.5/3 i.e. Good. Streetlights have been provided throughout the stretch. In the lane adjoining the Humayun's Tomb Complex, streetlights provided are placed too far from each other, resulting in low illumination. On Lodhi Road in addition to the streetlights on the central median, pedestrian scale lighting is provided for the cycle track and footpath. However, they were found to be non - operational at the time of audits.



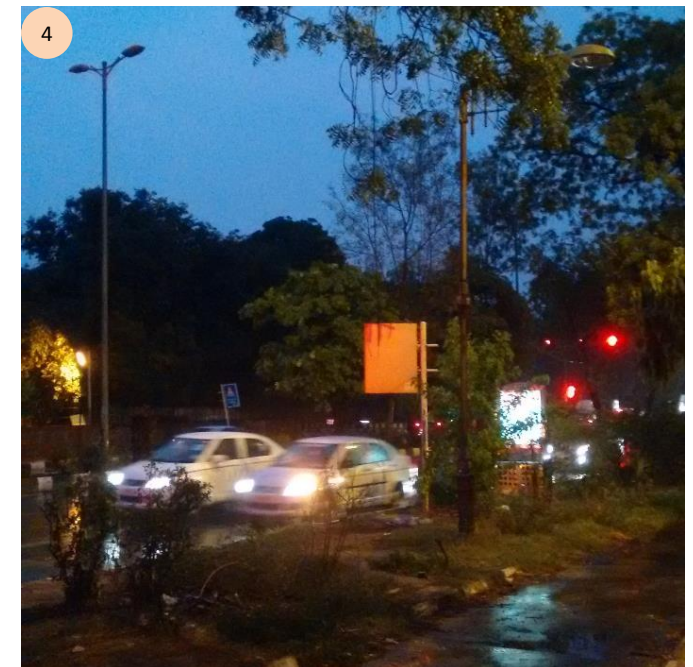
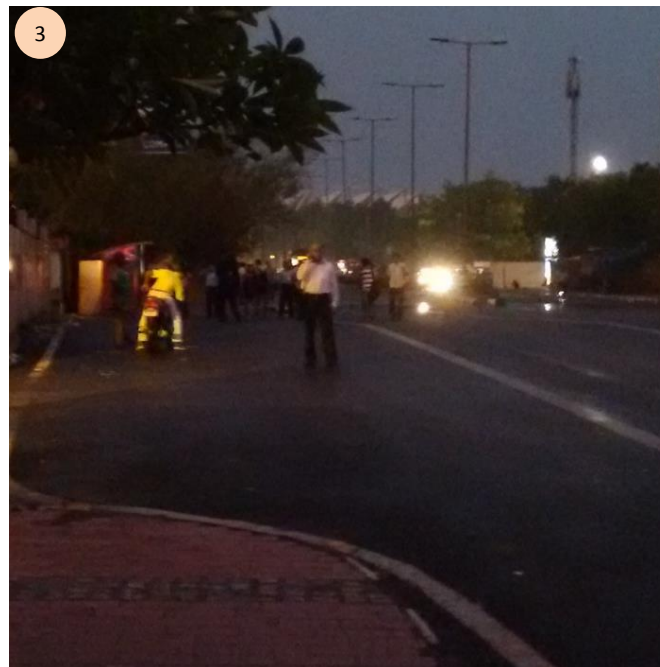
Map showing lighting rating

1 *There are no streetlights installed near the second entry to the tourist place. Also, lighting poles on this road i.e. Bharat Scouts and Guide Marg are installed too far from each other, resulting in low lighting. Therefore, additional light poles should be installed along this stretch.*

2 *As seen in the image, the streetlights have been provided on the central median only thereby, focusing on the vehicular carriage way of the road. This results in poorly lit footpath and cycle track. Pedestrian scale lights should be installed on the edge of the footpath, thereby illuminating both the footpath and the cycle track.*

3 *Some streetlights near the bus stop (image 3) and on Lodhi Road (image 4) were found to be non- operational at the time of audits.*

4 *They should be made operational. Also, the foliage along the streetlights needs to be pruned regularly as the overgrown leaves shield the light fixture resulting in poor illumination*



VISIBILITY

Visibility is rated 0.7 i.e. Below Average. There are some vendors near the entry/exit gate of the Humayun's Tomb Complex but there is no designated space for them. The lane adjoining the complex is largely inactive due to no "eyes on the street". Additionally, with high boundary walls and large building setbacks, Lodhi Road offers minimal visibility for a pedestrian.



Map showing Visibility rating

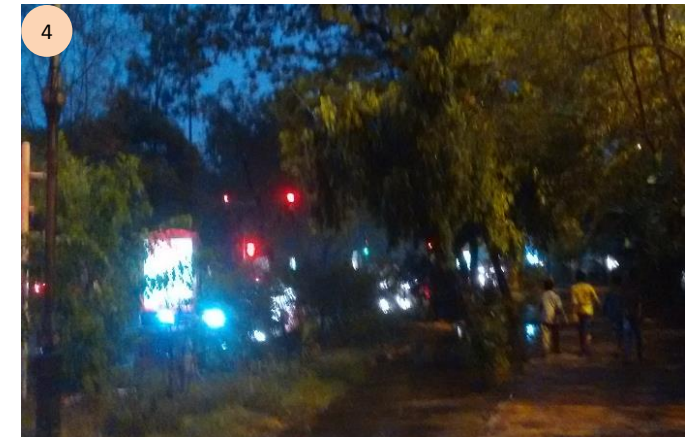
1 As seen in the image, the stretch near the complex's second entry, is largely inactive. There is a parking space for private vehicles, and a few vendors are seen behind the parked vehicles in image 2. To make this stretch active, a hawker zone should be set up here equipped with street furniture. Also, as discussed before, additional lighting is needed to avoid dark spots.



3 Lodhi Road offers poor visibility along most of its stretch, however there are few vendors near the roundabout. Apart from a wide footpath, it also has a cycling track but obstructed by parked cars. To enhance this space, a hawker zone should be created equipped with street furniture along the boundary wall, Also, parking should be disallowed in cycling track.



4 The stretch seen in the image is flanked by trees and shrubs from both sides. The height of the shrubs should be maintained 1 m. With proper lighting and uninterrupted cycling track, this stretch can be enhanced.



INDIA GATE

Safety Score: 3.5 / 5

About

India Gate is a war memorial, located on eastern end of the 'ceremonial axis' of New Delhi i.e. Rajpath. It is a memorial to the soldiers of Indian Army who sacrificed their lives in the First World War. Another memorial, Amar Jawan Jyoti was added much later after India got its independence.

Social Usage

Surrounding the imposing structure is a large expanse of lush green lawns, which is a popular picnic spot. One can see large number of people moving about the brightly lit area and on the lawns in evenings. Numerous street vendors are present in and around this place. During national holidays, India Gate and its surrounding gardens are closed for security reasons.

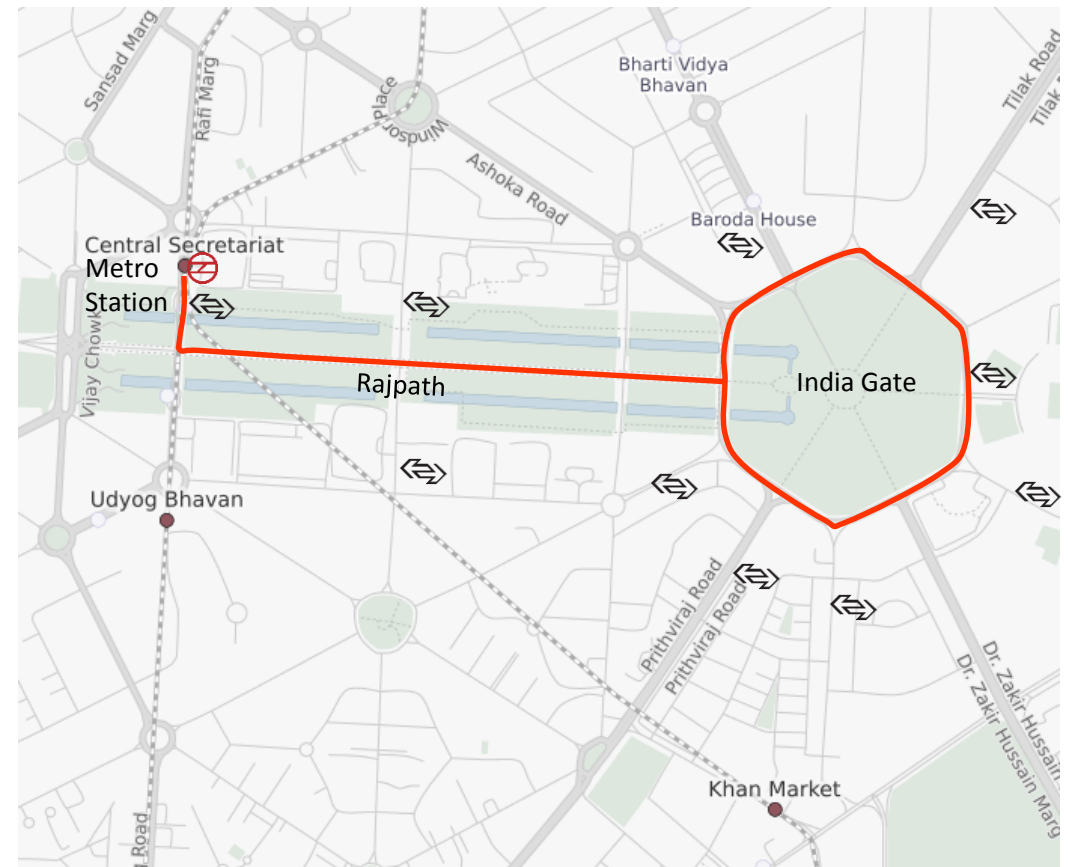


India Gate

Safety Audit

The route from Central Secretariat metro station to India Gate i.e. approximately 1.7 km has been audited and 105 audit pins have been generated. The nearest bus stop is 500 m away from India Gate. The area outside the metro station entry/exit and the various routes to India Gate were studied.

Nearest Metro Station – Central Secretariat (Yellow + Violet Line)



Location of the tourist spot



Metro Station












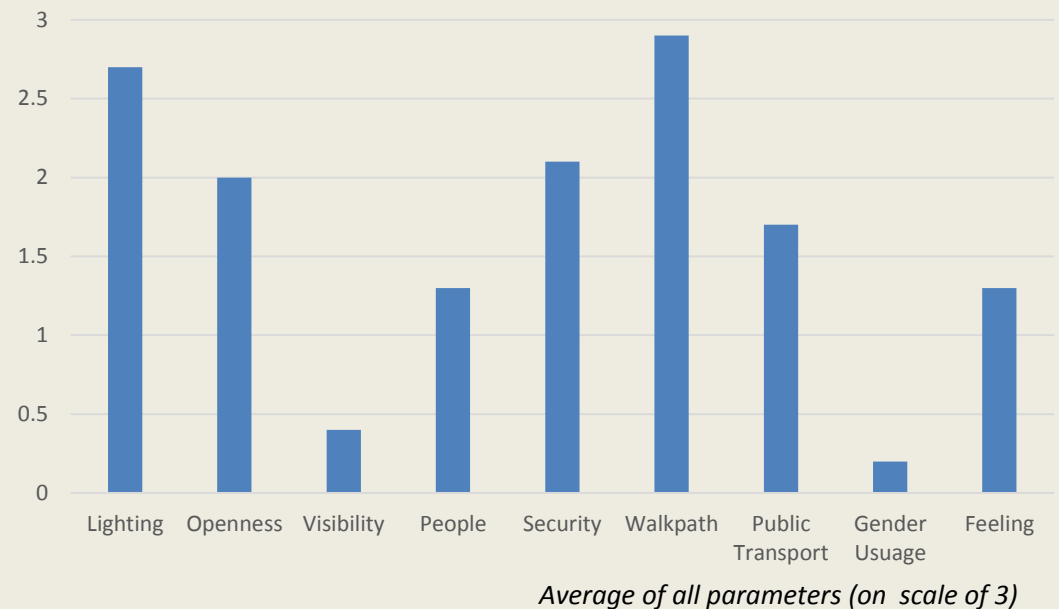
Bus Stop



Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	2.7 Good Street lights are provided throughout the stretch, however some were found to be non-operational at the time of audits.
 Openness	2.0 Above Average India Gate is flanked by huge gardens all around that offers clear line of sight all around.
 Visibility	0.4 Poor The numerous street vendors are present only around India Gate and its parks. At the roundabout, the high boundary walls offer poor visibility.
 People	1.3 Average Pedestrian count decreases as we move away from the Rajpath, towards the roundabout and surrounding areas.
 Security	2.1 Above Average Police patrolling van is present at the various entry points to the India Gate .
 Walk Path	2.9 Good Footpath is provided throughout the stretch and is in good condition.
 Public Transport	1.7 Average Autos are easily available near India Gate. Bus Stops are present at regular distance.
 Gender Usage	0.2 Poor Women and children's movement was found to be limited to the India Gate, it's parks. And on Rajpath.
 Feeling	1.3 Below Average While the India Gate and its adjoining parks are active till late night, the roundabout and the surrounding areas are secluded.

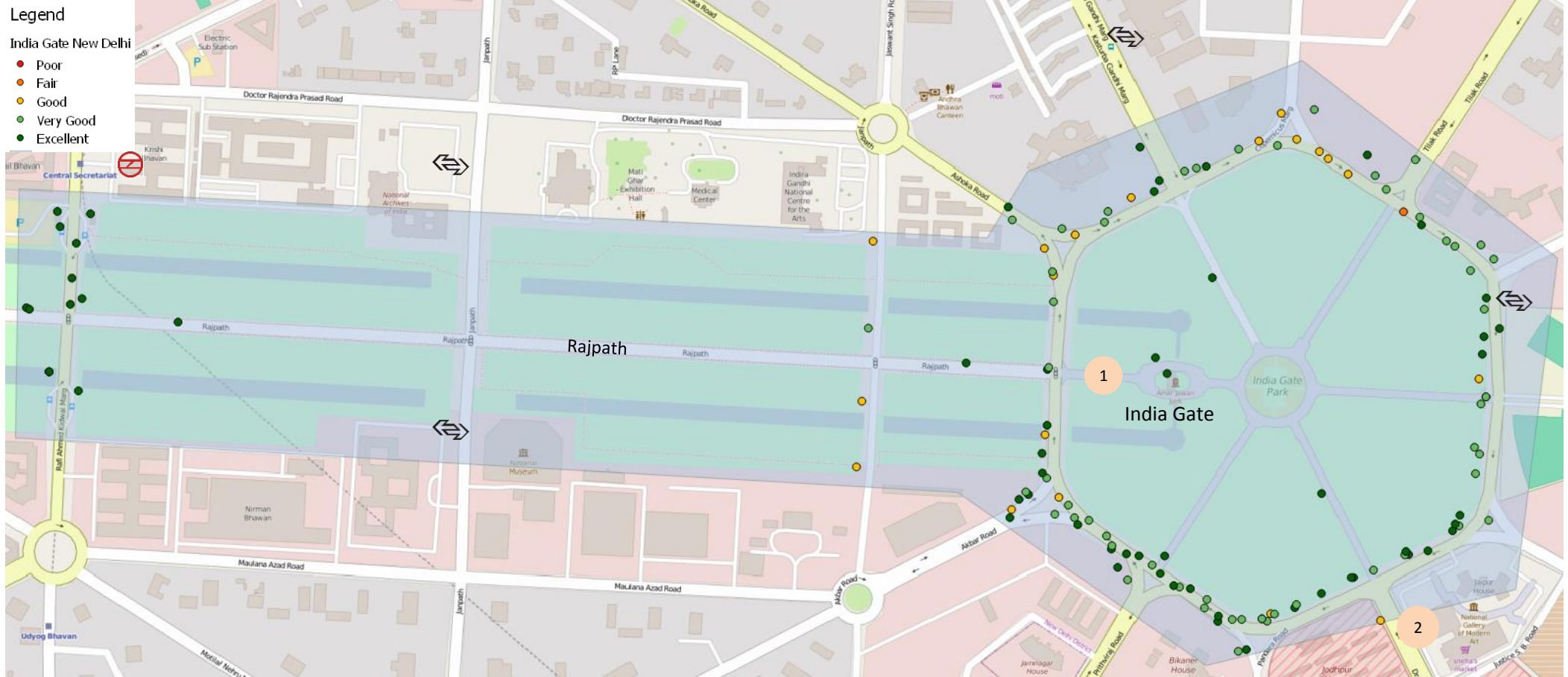
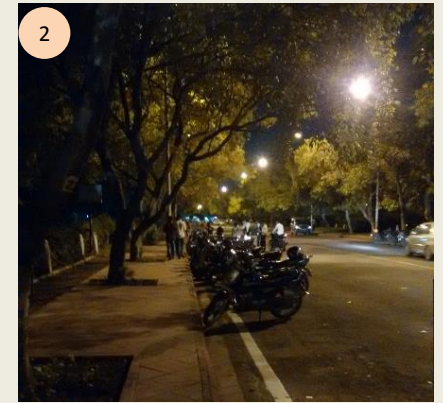


The path leading to India Gate

SAFETY SCORE

Safety Score is excellent at audit point (image 1) owing to high score in Lighting, Walkpath, People, Gender Usage, Visibility and Feeling.

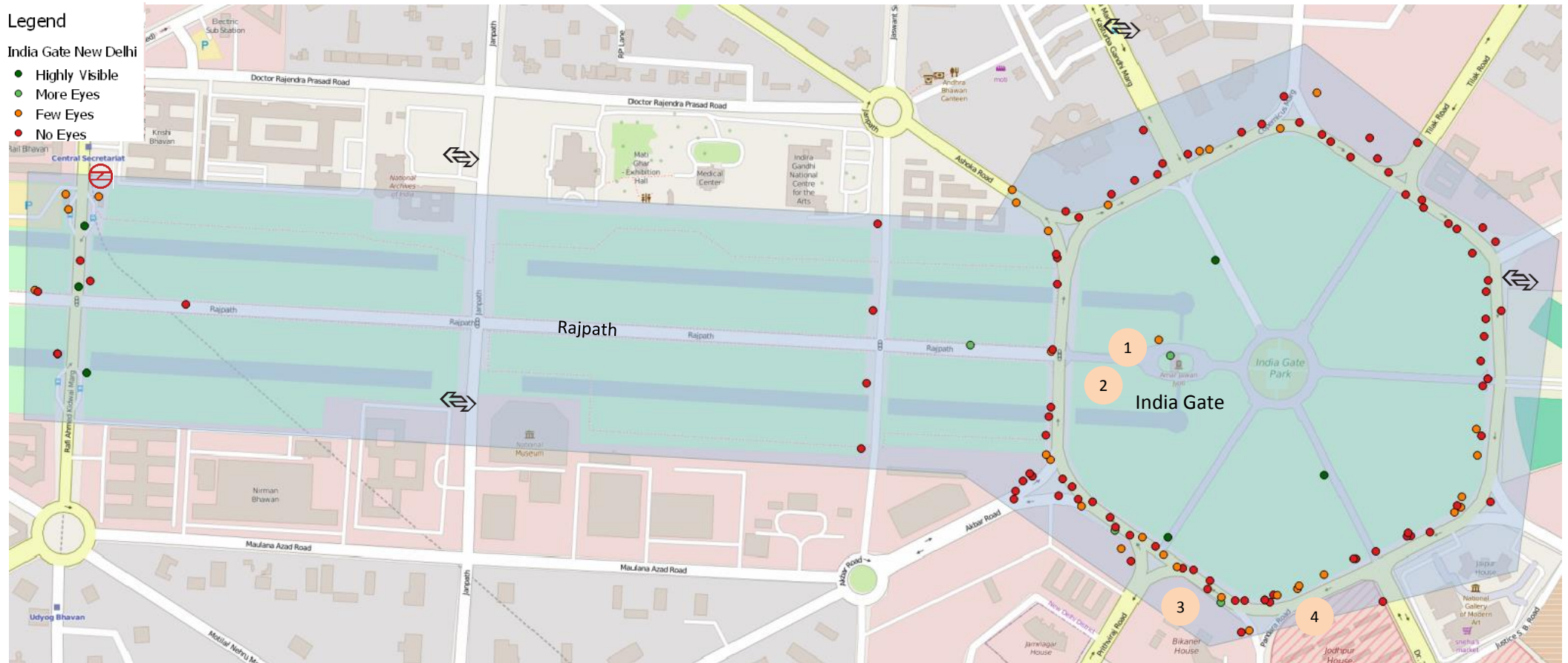
Safety Score is fair at audit point (image 2) due to low rating in Visibility, Security and Feeling.



Map showing Safety Score rating

VISIBILITY

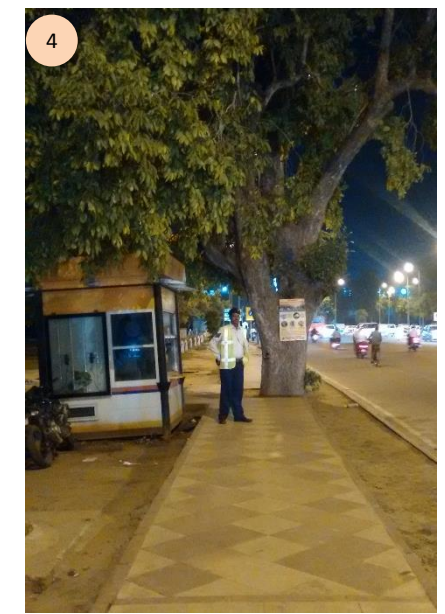
Visibility is rated 0.4 i.e. Poor. The India Gate roundabout is flanked by various Government buildings. With high boundary walls and no street vendors, this part of audit stretch offers low visibility. On the other hand, there are numerous street vendors at the India Gate, it's parks and Rajpath.



Map showing visibility rating

1 As seen in image 1 and 2, numerous street vendors can be seen near the India Gate on Rajpath. This space can be further enhanced by providing street furniture for people and the street vendors.

3 The vehicular roundabout of India Gate is flanked by boundary walls on one side and parks on the other. To improve visibility on this stretch, vending kiosks can be set up, clear of the footpath as seen in image 4.



JANTAR MANTAR

Safety Score: **3.5 / 5**

About

Jantar Mantar is located in central Delhi. Built by Maharaja Jai Singh II of Jaipur, who built similar structures in Jaipur, Varanasi, Mathura and Ujjain. One of the remarkable aspects of Jai Singh's observatories is that each site is distinctly different in size, layout, and style. The observatory consists of large astronomical instruments namely Misra Yantra; Samrat Yantra, Jai Prakash, and Rama Yantra.

Social Usage

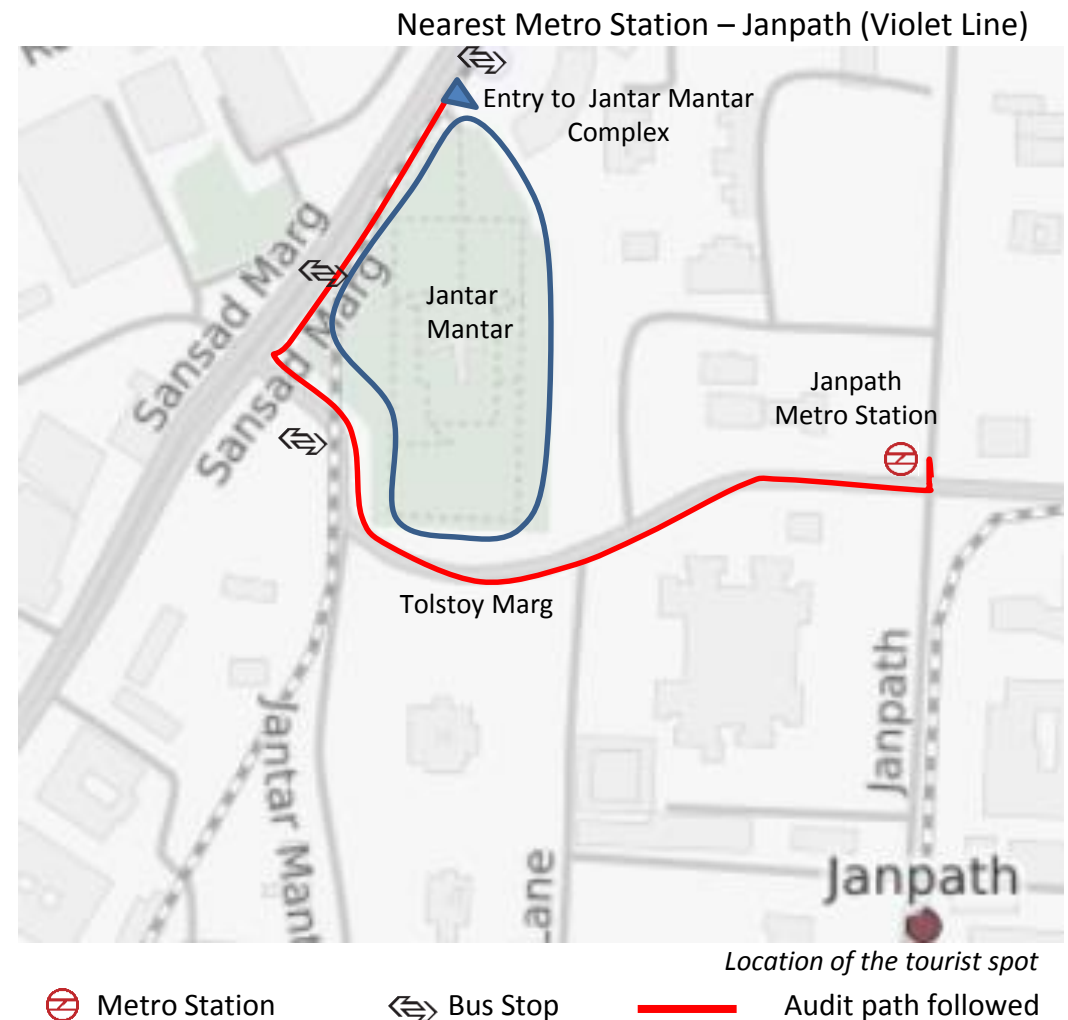
Surrounded by various government offices and cultural center, the observatory is a major tourist attraction. The instruments at Jantar Mantar are fascinating for their ingenuity, but accurate observations can no longer be made from here because of the tall buildings around. Situated in a strategic location, the area is known for staging protests and dharna regarding social and political issues.












Jantar Mantar

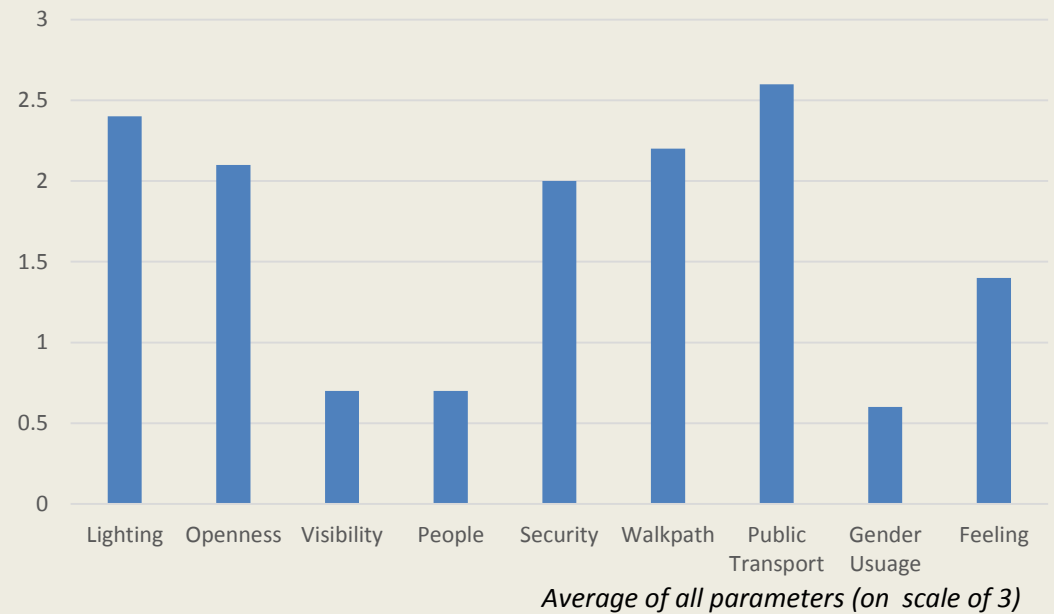
Safety Audit

The route from Janpath metro station to Jantar Mantar i.e. approximately 600m has been audited and 29 audit pins have been generated. The nearest bus stop is 100 m away from the entry/ exit of Jantar Mantar. The area outside the metro station entry/exit and the route to Janpath were studied.



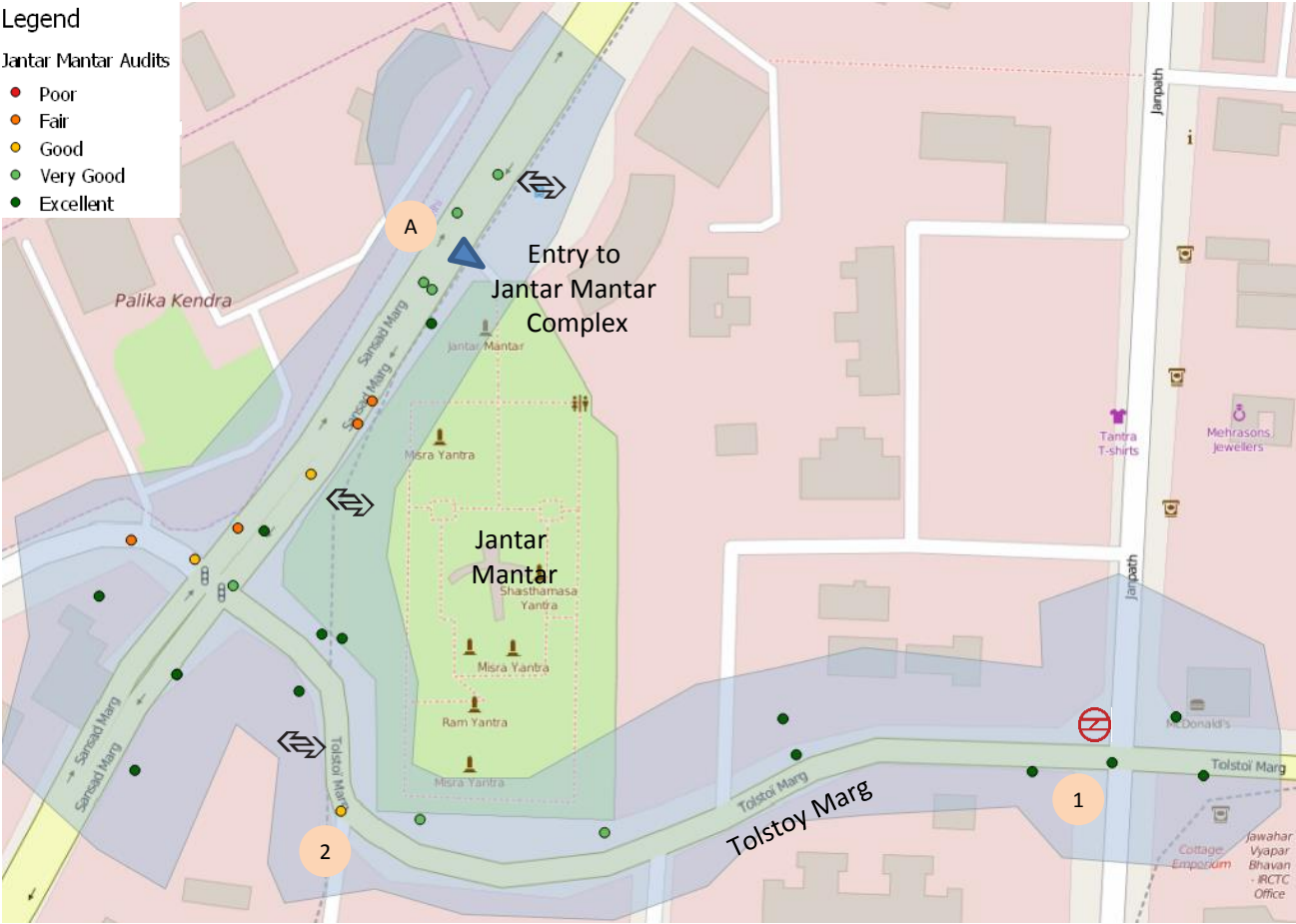
SAFETY AUDITS ANALYSIS

 Light (Night)	2.4 Above Average The streetlights are provided throughout the stretch, and are in working condition.
 Openness	2.1 Above Average As a part of Lutyens' Delhi, the area has wide roads which offers clear line of sight.
 Visibility	0.7 Below Average Visibility is rated poor with high boundary walls and huge set backs of government buildings.
 People	0.7 Below Average People is rated below average since pedestrian count is low after dark
 Security	2.0 Above Average Regular Police patrolling is done and a police station is present on the audit stretch.
 Walk Path	2.2 Above Average Footpath is provided throughout the audit stretch and is in good condition.
 Public Transport	2.6 Good Bus Stops are at regular distance and autos are easily available.
 Gender Usage	0.6 Poor Gender Usage is rated poor as few women and children were seen during the time of audits.
 Feeling	1.4 Average Overall, feeling is rated average.



The area surrounding Jantar Mantar

SAFETY SCORE



Map showing Safety Score rating



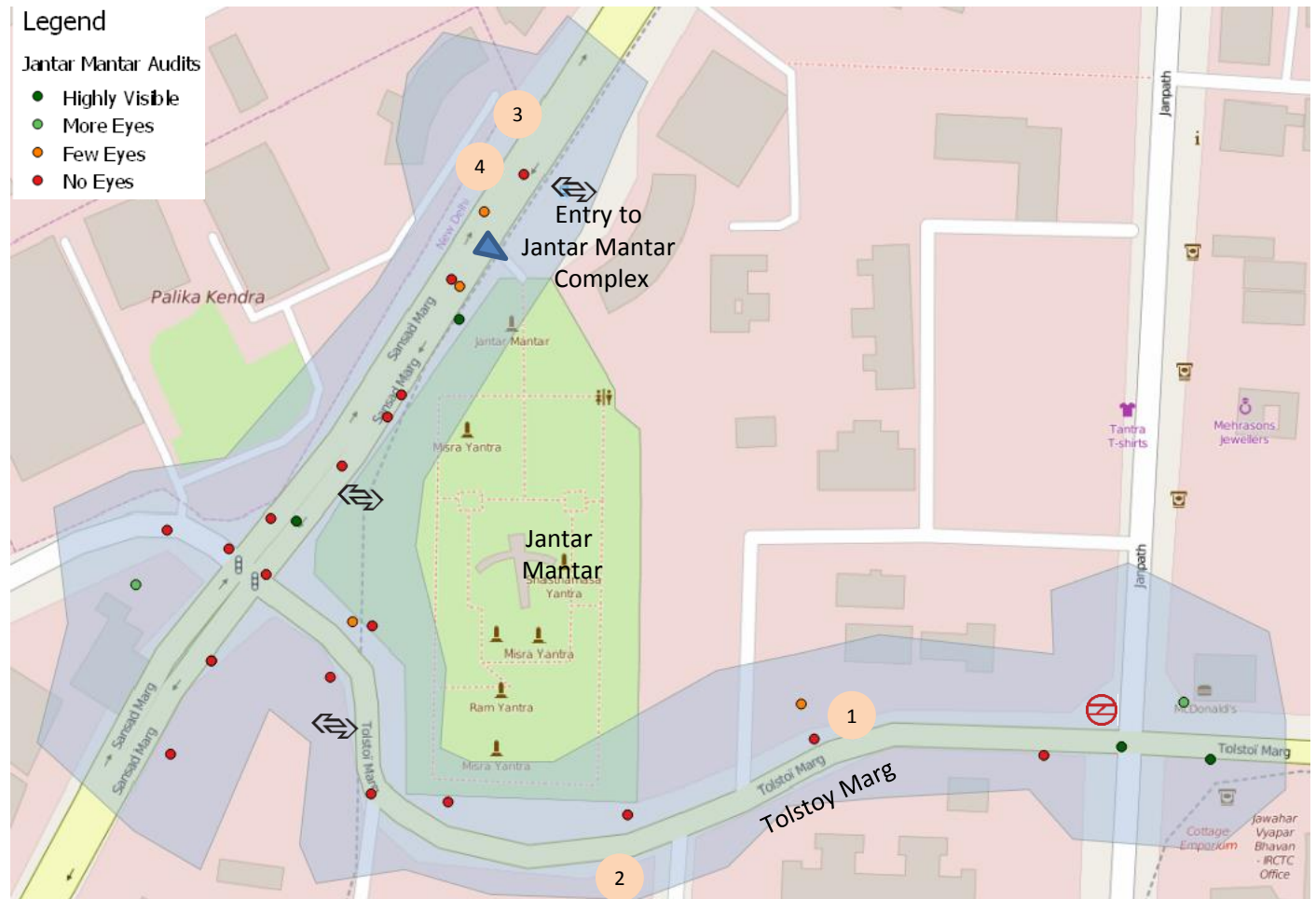
Janpath has been rated excellent in Safety score owing to high score in Visibility, People, Walkpath, Lighting, Public Transport and Feeling parameters.



This spot on Tolstoy Marg has been rated fair in Safety score owing to low score in Visibility, Security, Gender Usage and Feeling parameters.

Visibility

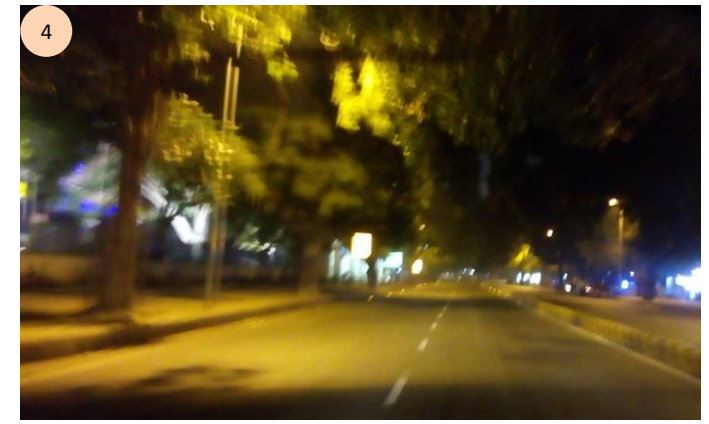
Visibility is rated 0.7 i.e. Below Average. Jantar Mantar is situated amidst various government offices. With high boundary walls and low number of street vendors, this stretch offers low visibility to the pedestrians. Also, Jantar Mantar and surrounding offices closes by 6 PM, thus the area becomes inactive at night.



Map showing Visibility rating

1 These stretch on Tolstoy Marg, with high boundary walls are inconvenient and unsafe for a pedestrian, be it a day or night. To improve visibility, the height of the solid part of boundary wall should be limited to 1 m and rest of the height should be achieved using grills. Also, a Public Convenience facility is needed along this stretch.

3 As seen in image 3 and 4, the environment of same stretch changes from day to night. While it is quite active during the day, it becomes secluded during the night. Since, this area is a part of built heritage, night tourism can be promoted here through heritage walks, photo walks, food walks, etc. This would ensure activity in these areas and could contribute to the feeling of safety.



LOTUS TEMPLE

Safety Score: **3.8** / 5

About

The Lotus Temple located in Delhi, is a Bahai house of worship which was completed in 1986. The temple is composed of 27 free-standing marble-clad "petals" arranged in clusters of three to form nine sides, with nine doors opening onto a central hall with height of slightly over 40 meters and a capacity of 2,500 people. The temple has been awarded with numerous awards for its notable shape and architecture.

Social Usage

Kalkaji Mandir, an ancient temple dedicated to Goddess Kali is located at a few distance from the Lotus Temple. However, with only temple and a park, the area is largely secluded and gets inactive in evening. A Police patrolling van is present throughout the day around this area.

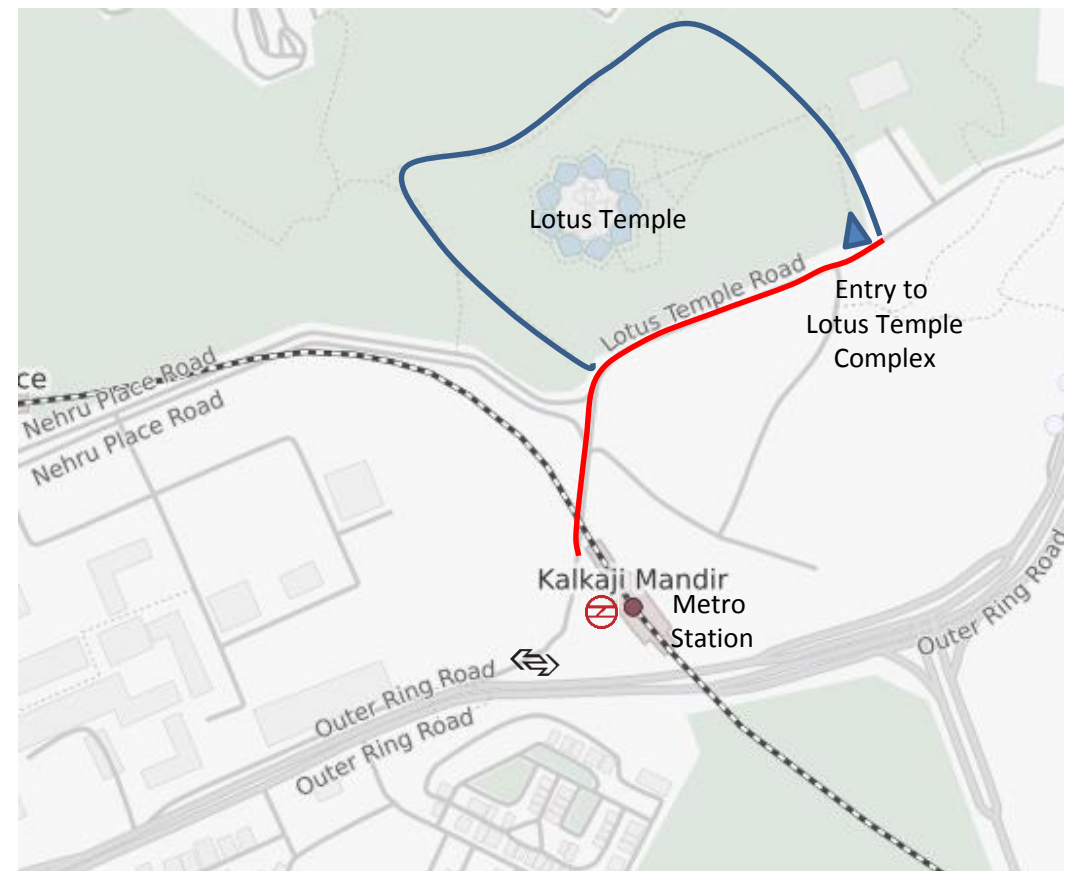


Lotus Temple

Safety Audit

The route from Kalkaji Mandir metro station to Lotus Temple i.e. approximately 550m has been audited and 35 audit pins have been generated. The nearest bus stop is located at a distance of 750 m from the tourist place. The area outside the metro station entry/exit and the route to the temple were studied.

Nearest Metro Station – Kalkaji Mandir (Violet Line)












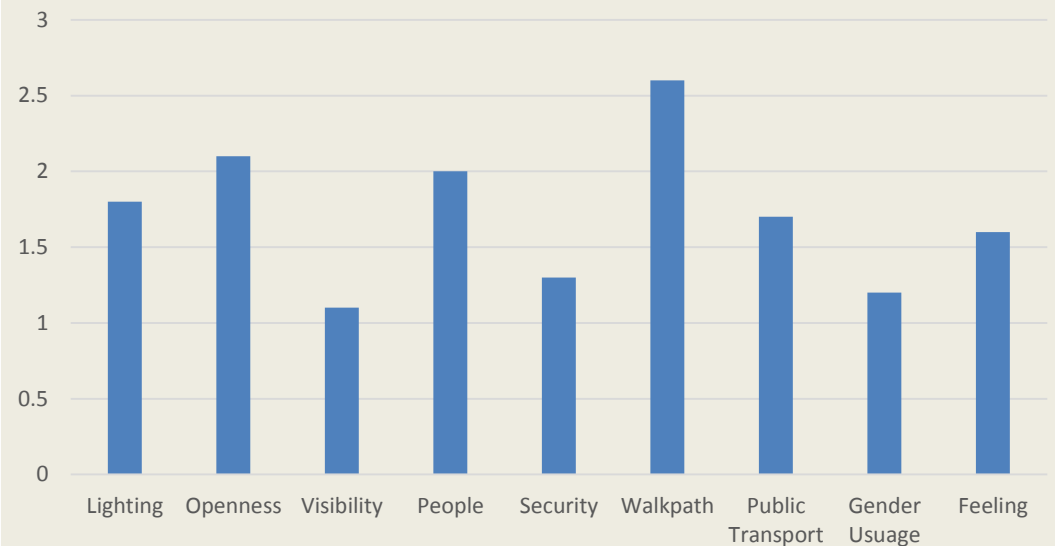
⊗ Metro Station

⇔ Bus Stop

Location of the tourist spot
— Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	1.8 Average Street lights are provided along the whole stretch but some were found to be non-operational.
 Openness	2.1 Above Average It is a four lane wide road with Lotus Temple and Kalkaji District Park in its surroundings.
 Visibility	1.1 Below Average Visibility is rated poor due to absence of any hawkers/ activity post sunset.
 People	2.0 Average People visiting Lotus Temple and Kalkaji Mandir contribute to the pedestrian count.
 Security	1.3 Average A Police Patrolling van is present throughout a day on this stretch.
 Walk Path	2.6 Good Footpath is provided throughout the stretch but is broken at places due to construction of Metro.
 Public Transport	1.7 Average Autos are easily available near the metro station till late evening.
 Gender Usage	1.2 Below Average Gender Diversity decreases in the evening.
 Feeling	1.6 Average With a District Park and a temple in its surroundings, feeling is rated average in this area.



Average of all parameters (on scale of 3)

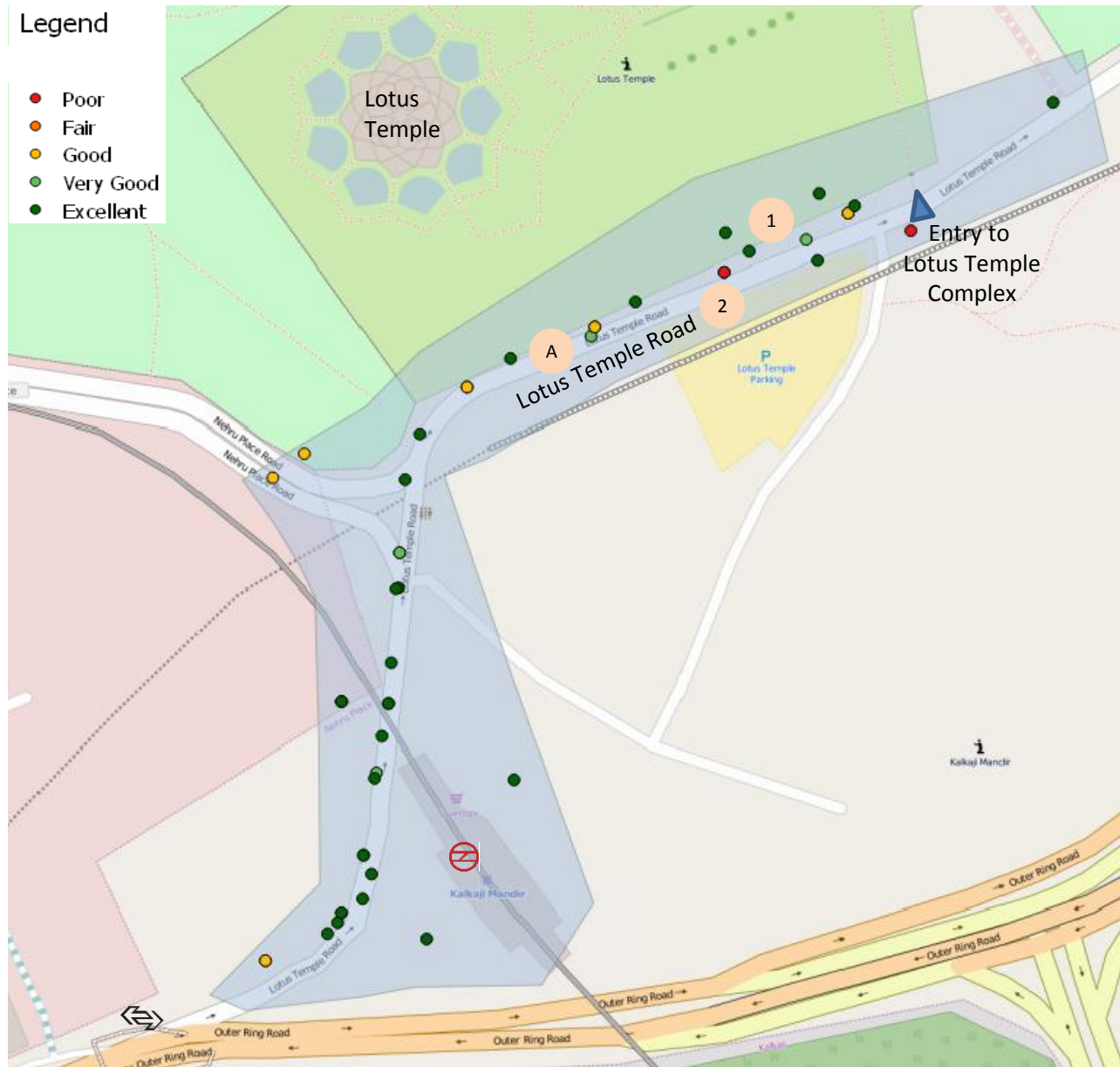


The path leading to Lotus Temple

SAFETY SCORE

Legend

- Poor
- Fair
- Good
- Very Good
- Excellent



Map showing Safety Score rating



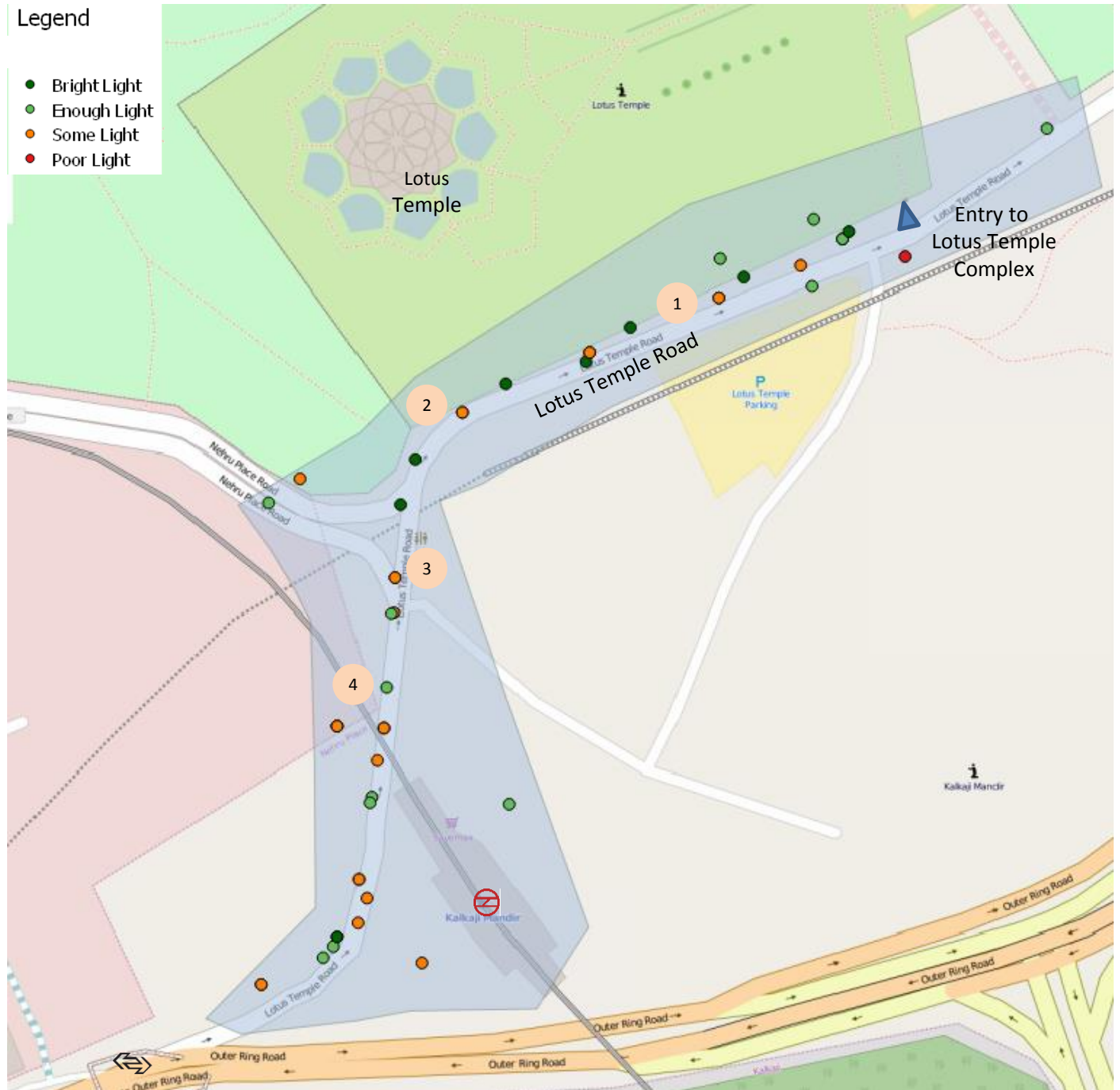
This audit point has high safety score based on high rating in Lighting, Visibility, People, Security, Gender Usage and Feeling parameters.



This spot near the parking lot has been rated poor in safety score owing to low rating in Lighting, Visibility, People, Security, Gender Usage and Feeling parameters.

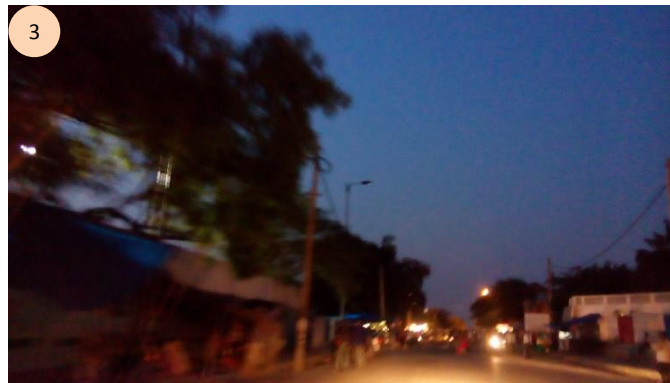
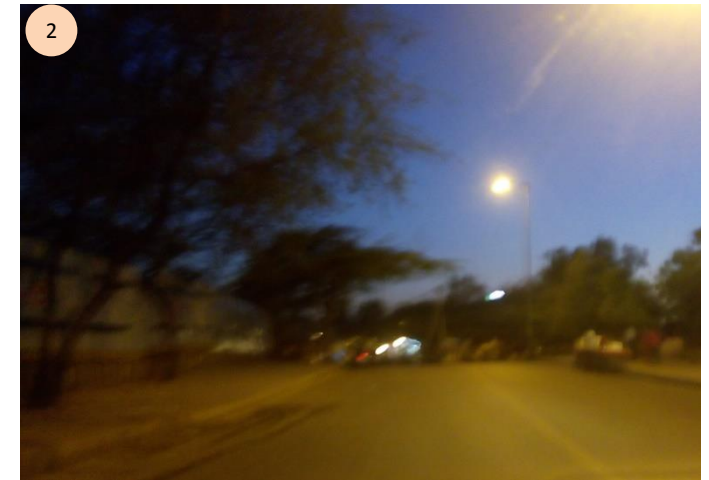
LIGHTING

Lighting is rated 1.6/3 i.e. Average. Some streetlights were found to be non operational. They need to be checked. On Lotus Temple road, streetlights are installed only on one side of the road. Given the width of the road, this results in inadequate level of lighting.



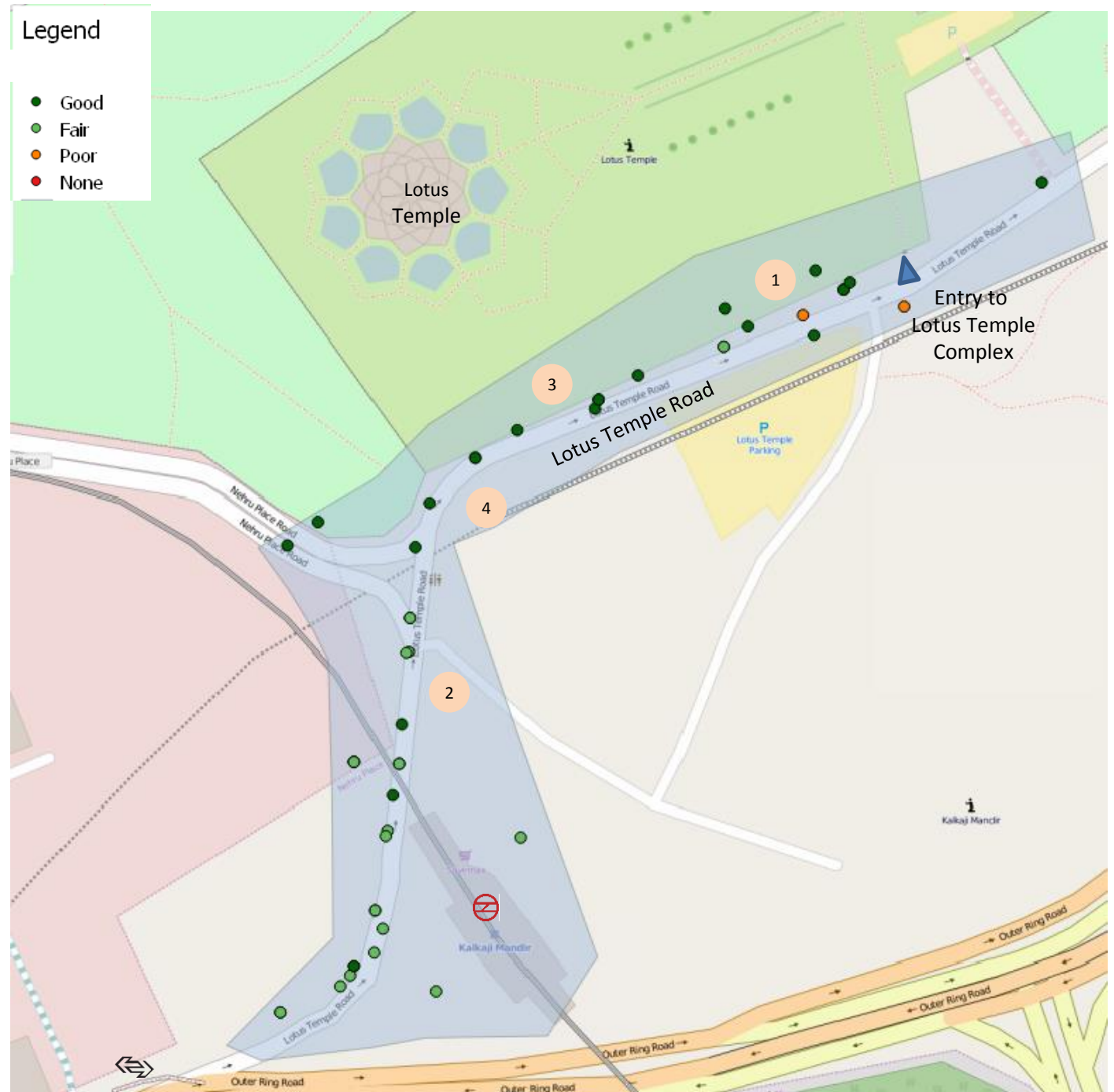
Map showing lighting rating

- 1 *The streetlights are installed only one side of the road. As a result, the footpath on the other side is poorly lit. To improve lighting on this stretch, additional streetlights should be installed on the other side, along the built edge facing the footpath and the road.*
- 2 *Some streetlights were found to be non – operational at the time of the audits. These need to be checked and repaired.*
- 3 *The space under the bridge (metro line) is poorly lit. Lighting from temporary stalls provide some illumination. However, it is inadequate, and not a permanent source of lighting. Therefore, additional streetlights should be installed to illuminate this space.*



WALKPATH

Walkpath is rated 2.6/3 i.e. Good. Footpath exists throughout the audit stretch but is broken and obstructed at few spots. Temporary stalls and street vendors have occupied the footpath near the metro station, forcing pedestrians to walk on the road.



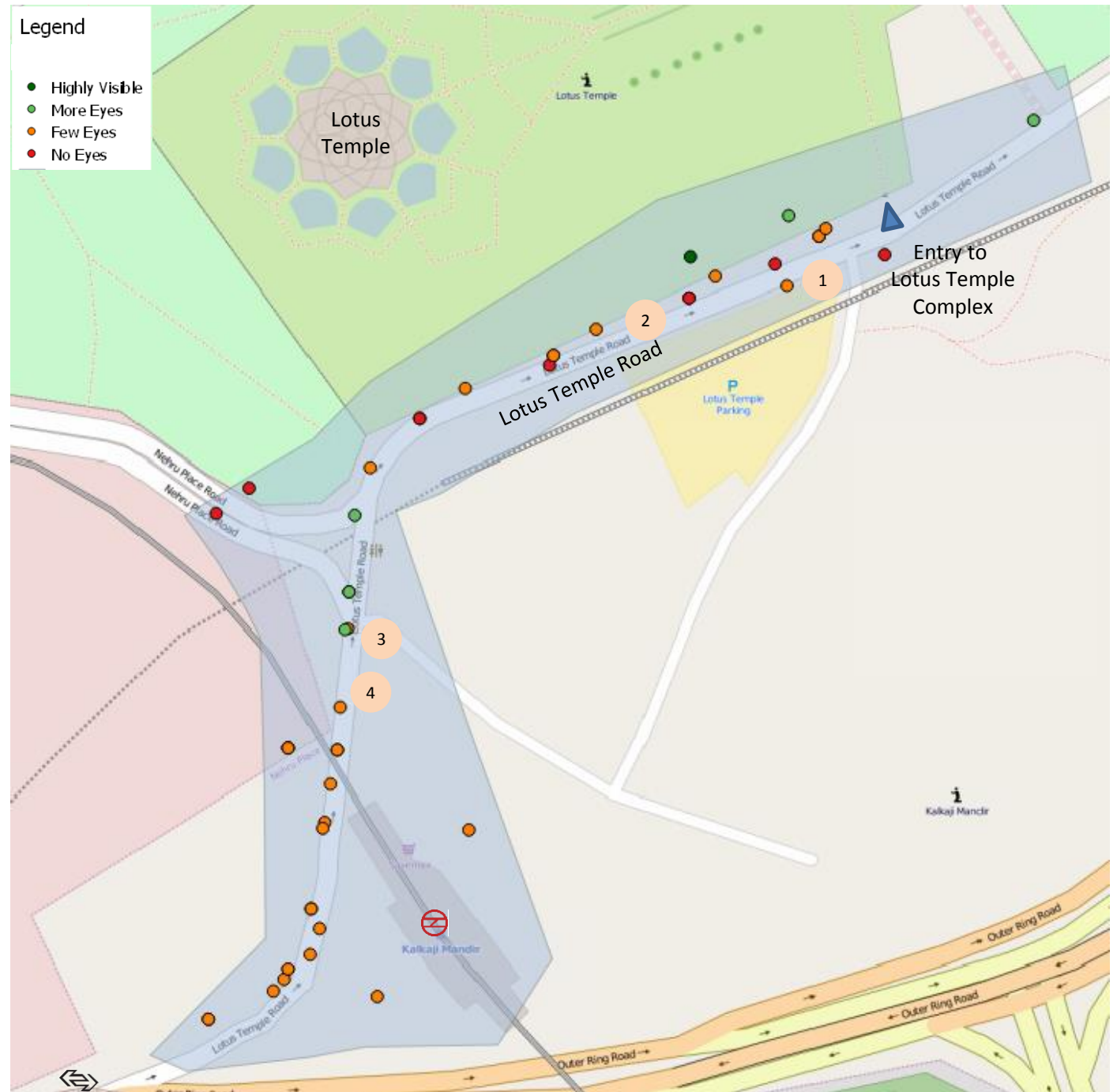
Map showing walkpath rating

- 1 Footpath exists throughout the stretch but is obstructed by temporary stalls and street vendors near the temple (image 1) and the metro station (image 2). A designated space should be provided for these hawkers, clear of the footpath. Also, it should be equipped with the street furniture for pedestrians.
- 3 Improperly placed street furniture like streetlights, dustbins as seen in the image 3 isn't favourable for the pedestrians. The streetlights should be installed along the boundary wall clear of the footpath. Also, the dustbin should be placed in designated hawker zone or by creating niches in the boundary wall, clear of the footpath.
- 4 The stretch from Lotus Temple to the metro station has a broken footpath on one side. Moreover, it is obstructed by construction debris and a transformer. The footpath should be repaired and widened wherever it is obstructed by the electric pole.



VISIBILITY

Visibility is rated 1.1 i.e. Below Average. The stretch is flanked by high boundary walls with temporary stalls and street vendors in between. With no designated space for the hawkers, they end up occupying the footpath. However, the stretch becomes inactive after sunset.



Map showing Visibility rating

1 As mentioned earlier, there are street vendors present near the entry/exit of the Lotus Temple during the day. In absence of a designated hawker zone, they occupy the footpath and road. A hawker zone should be created, equipped with street furniture for people to sit and wait. Currently, the area gets secluded and inactive after sunset. To make this stretch active, some activities or festivals can be organized in the adjoining parks namely Astha Kunj Park and Kalkaji District Park.

3 Outside Kalkaji Metro Station, there are street vendors catering to metro commuters as well as devotees visiting Kalkaji Mandir. A hawker zone should be set up, clear of the footpath. It should have a public convenience, and street furniture.



PURANA QILA

Safety Score: **3.5 / 5**

About

Purana Qila or Old Fort, is one of the oldest forts in Delhi. Some believe that the site of Purana Qila was perhaps that of Indraprastha, the legendary capital of the Pandavas. It's current form was built by Sher Shah Suri. Sher Shah Suri raised the citadel of Purana Qila with an extensive city-area sprawling around it.

Social Usage

Light and Sound shows are conducted in evening which goes till 9:30 – 10:00 PM. Adjoining the complex, there is a lake which is commercially used for boating. Also, present are National Zoological Park, Bhairon Mandir and Qila-e Kuhna Masjid surrounding the complex. The area gets inactive and secluded after dark. Police Patrolling van was seen to be patrolling on Mathura Road.

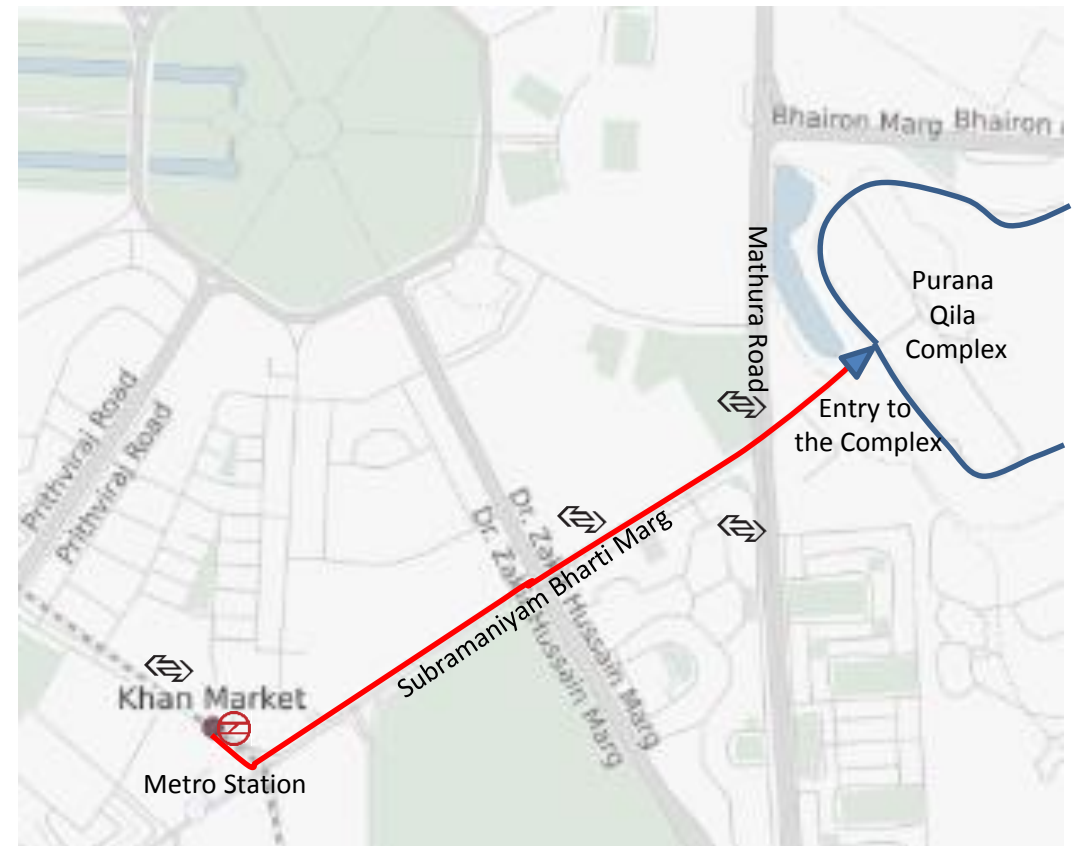


Purana Qila

Safety Audit

The route from Khan Market metro station to Purana Qila Complex i.e. approximately 1.5 km has been audited and 44 audit pins have been generated. The nearest bus stop is 300 m away from the tourist place. The area outside the metro station entry/exit and outside Purana Qila were studied.

Nearest Metro Station – Khan Market (Violet Line)



Location of the tourist spot



Metro Station












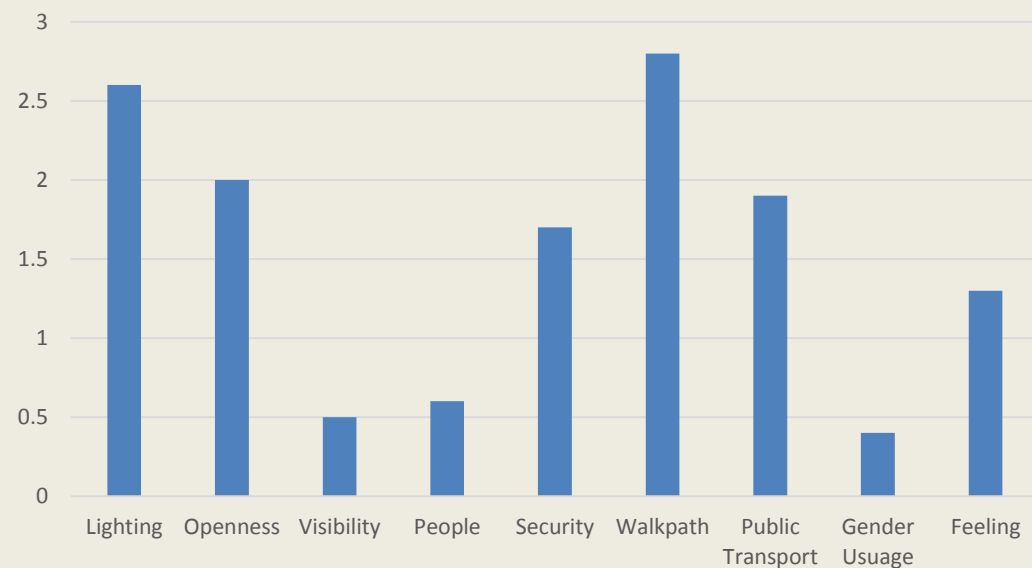
Bus Stop



Audit path followed

SAFETY AUDITS ANALYSIS

 Light (Night)	2.6 Good Street lights are provided throughout the stretch. However, they are placed too far from each other on the stretch from Mathura Road to Purana Qila Complex resulting in low illumination.
 Openness	2.0 Above Average The area has wide roads, giving a clear line of sight to the pedestrians.
 Visibility	0.5 Poor The stretch on Subramaniam Bharti Marg to Mathura Road is flanked by high boundary walls resulting in poor visibility on road.
 People	0.6 Poor Pedestrian count is poor along this audit stretch.
 Security	1.7 Average Private security was present outside the complex. Police patrolling Van was seen on Mathura Road.
 Walk Path	2.8 Good Footpath is provided but is broken and obstructed at a few locations.
 Public Transport	1.9 Above Average Bus stops are present at regular distance and autos are easily available.
 Gender Usage	0.4 Poor Gender diversity was found to be poor after dark.
 Feeling	1.3 Below Average Overall, feeling was rated below average by the auditors.

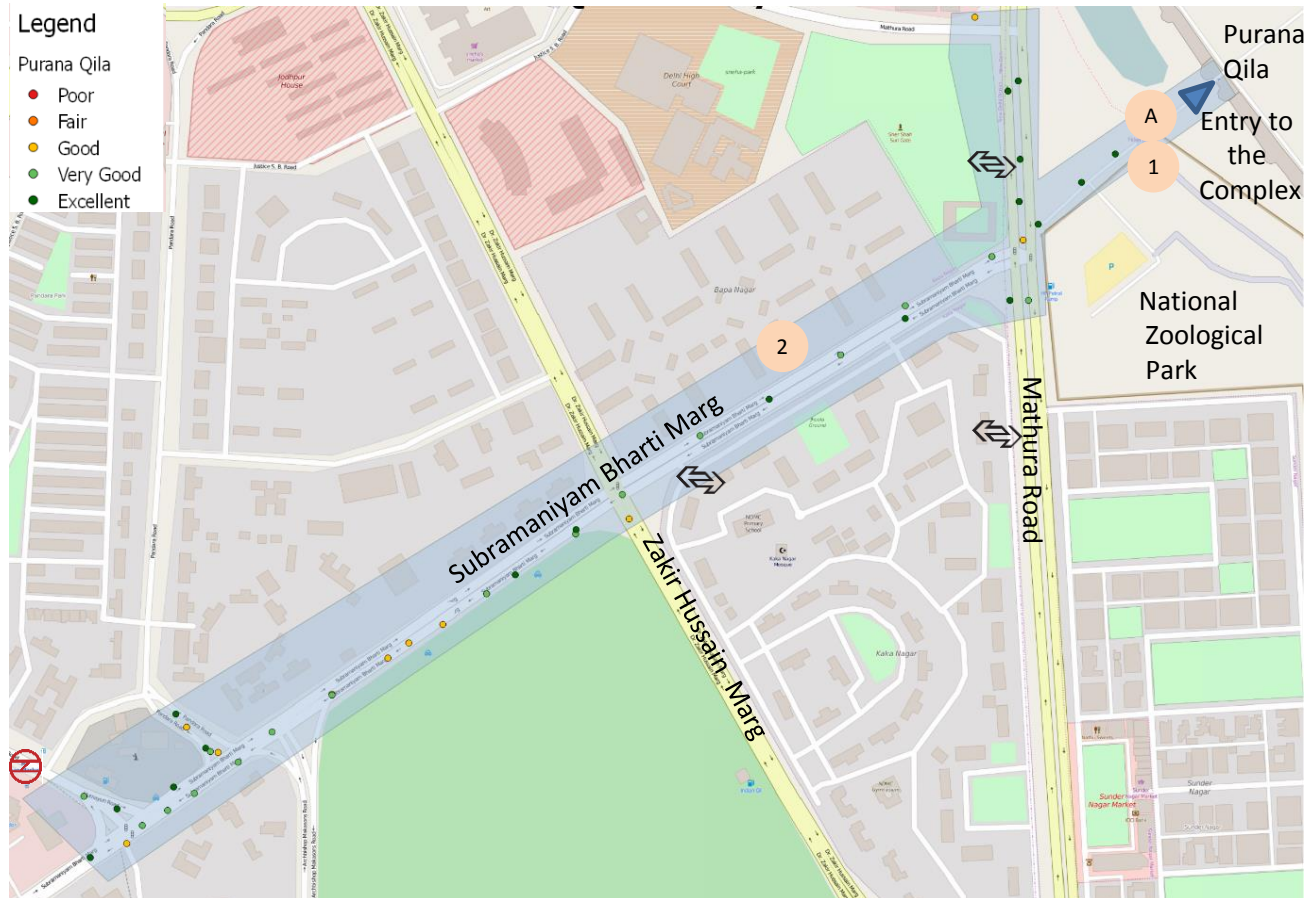


Average of all parameters (on scale of 3)



The path leading to Purana Qila

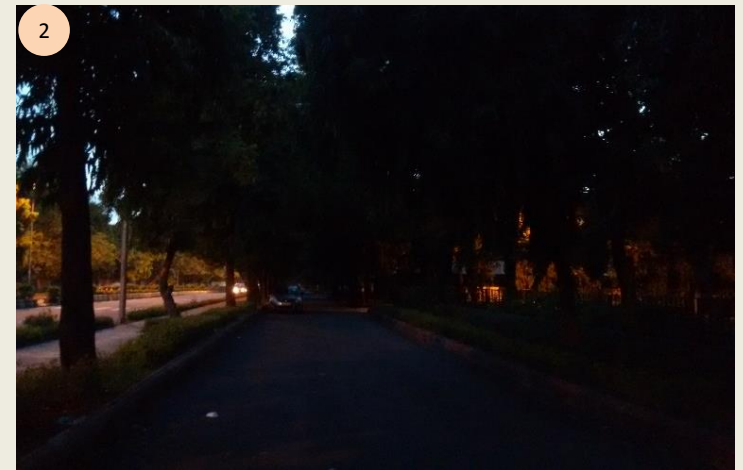
SAFETY SCORE



Map showing Safety Score rating



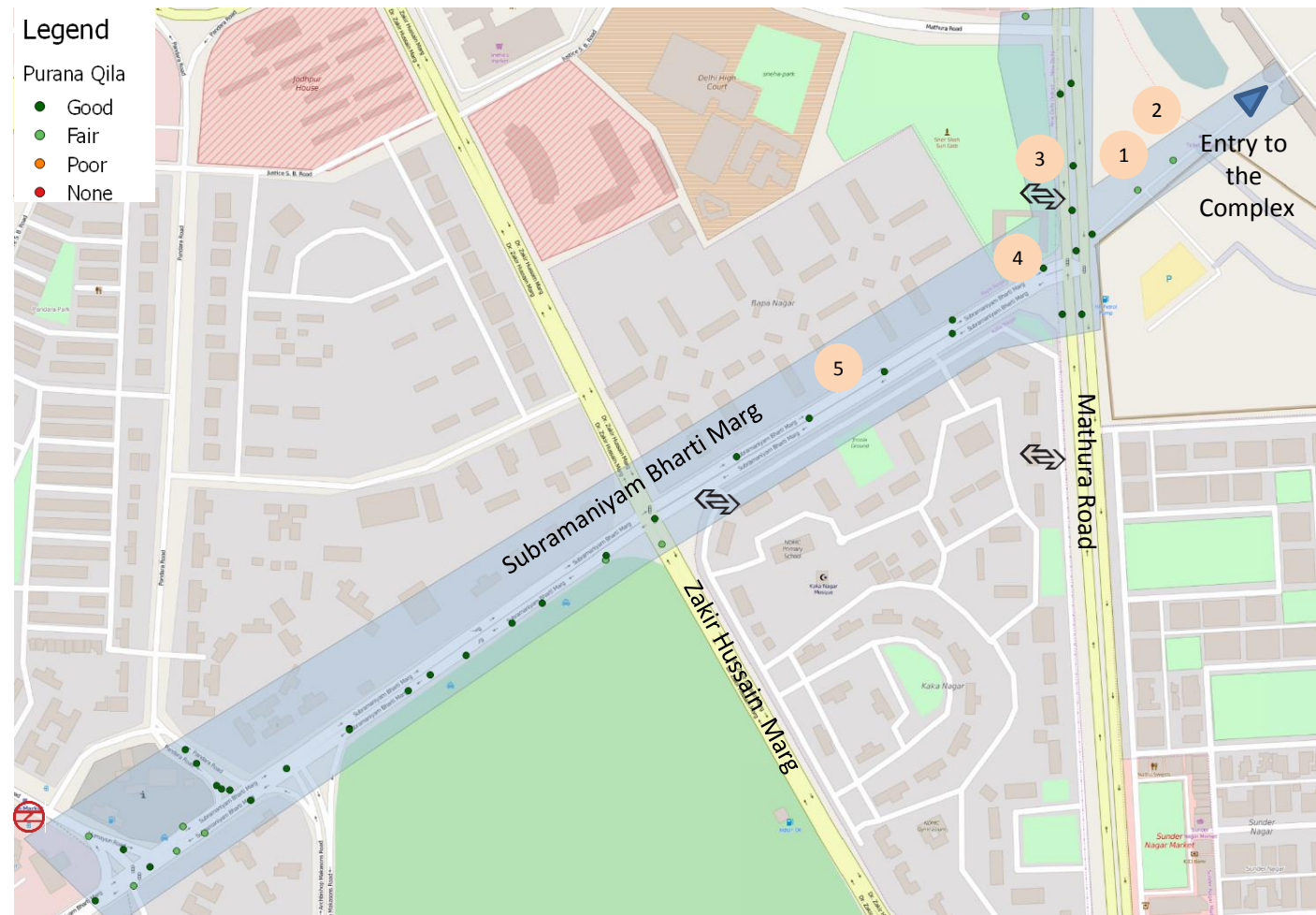
This spot has been rated high Public Transport, People, Gender Usage and Feeling parameters.



This spot on Subramaniam Bharti Marg has been rated low in Lighting, Visibility, People, Security, Gender Usage and Feeling parameters.

WALKPATH

Walkpath is rated 2.8/3 i.e. Good. This is so because footpath exists throughout the stretch. However, it is broken and non-continuous at few spots. Tactile paving done on few stretches end abruptly, thus proving risky for visually challenged people. At few places, footpath is obstructed by hawkers, construction debris, trees etc. These obstructions need to be removed to ensure smooth movement for the pedestrians.



Map showing walkpath rating

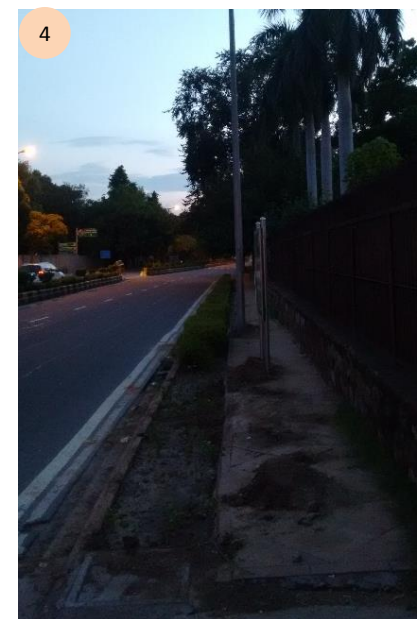
- 1 The footpath leading to Purana Qila complex is occupied by street vendors and vehicles are parked over it, forcing pedestrians to walk on vehicular carriageway. To ensure a smooth movement of the pedestrians, the footpath should be widened (till the planter in image 2) to accommodate one and all. Hawkers can be given space along the boundary wall, clear of the footpath. The vehicular parking should be restricted to the parking lot provided by Zoological Park and Qila complex authorities.



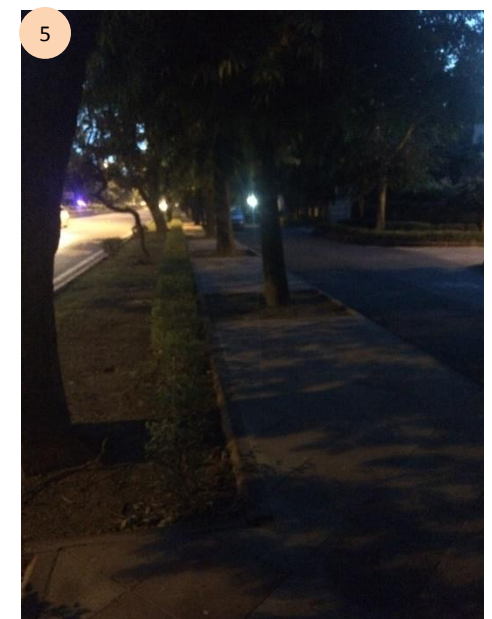
- 3 The tactile paving ends abruptly, which can prove risky for visually challenged people. The footpath level should be flushed with that of bus stop or provided a gradual slope. Also, a designated space for the hawkers should be provided, clear of the footpath.



- 4 The footpath is broken and obstructed by light poles and signage board (image 4) as seen on Subramaniam Bharti Marg. This should be removed and installed at the edge of the footpath and the footpath should be repaired.

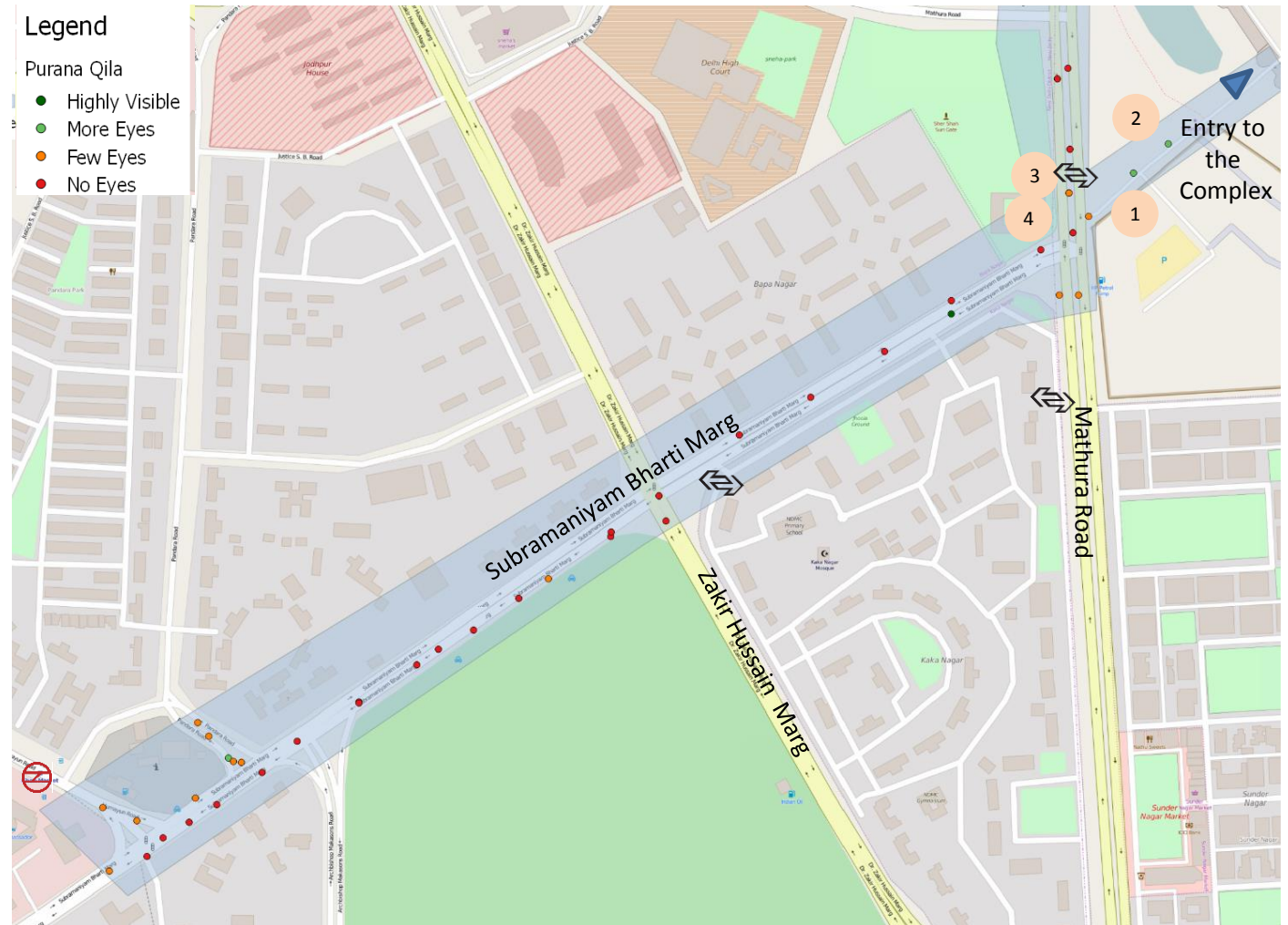


- 5 A footpath should be widened wherever it is obstructed by a tree. In this case, the footpath should also have pedestrian scale streetlights installed along its edge.



VISIBILITY

Visibility is rated 0.5/3 i.e. Poor. The stretch from metro station i.e. Subramaniam Bharti Marg to Mathura road is flanked by high boundary walls. As we move along Mathura road, we see few hawkers and temporary stalls occupying the footpath.



Map showing visibility rating

1 The stretch leading to Purana Qila Complex from Mathura Road gets inactive post sunset. This stretch is flanked by boundary walls of Qila complex on one side and Zoological Park on the other. Seen are few vendors occupying the footpath (image 2), and the autos lined up near the boundary wall (image 1). To enhance this area, a Para-Transit Hub should be created here. A designated auto stand should be set up with proper parking space for the autos on one side. As mentioned earlier, the footpath on the other side should be widened to create space for a hawker zone, equipped with street furniture for people to sit.

3 As seen in image 3 and 4, the path behind the bus stop is flanked by trees and shrubs on one side, and boundary wall on the other. To improve visibility, the tree leaves should be regularly pruned and height of the shrubs should be maintained at 1 m. Also, this space should be redesigned to provide a designated hawker zone for the existing hawkers present near the bus stop.



QUTUB MINAR

Safety Score: **3.7** / 5

About

Qutub Minar is located in Mehrauli area of South Delhi. With the height of 72 meters, it is the tallest brick minaret in the world. Qutub Minar, a UNESCO World Heritage Site is a part of Qutub Complex which has several other ancient and medieval monuments surrounding it. The Minar is made of bricks and decorated with intricate carvings and verses from the Quran.

Social Usage

The Qutub Complex is part of culturally significant area of Mehrauli. Mehrauli is one of the seven ancient cities that formed to make Delhi, as we see now. Cultural programmes like Qutub Festival is usually held around November–December in the Qutub complex by Delhi Tourism, Govt. of Delhi.

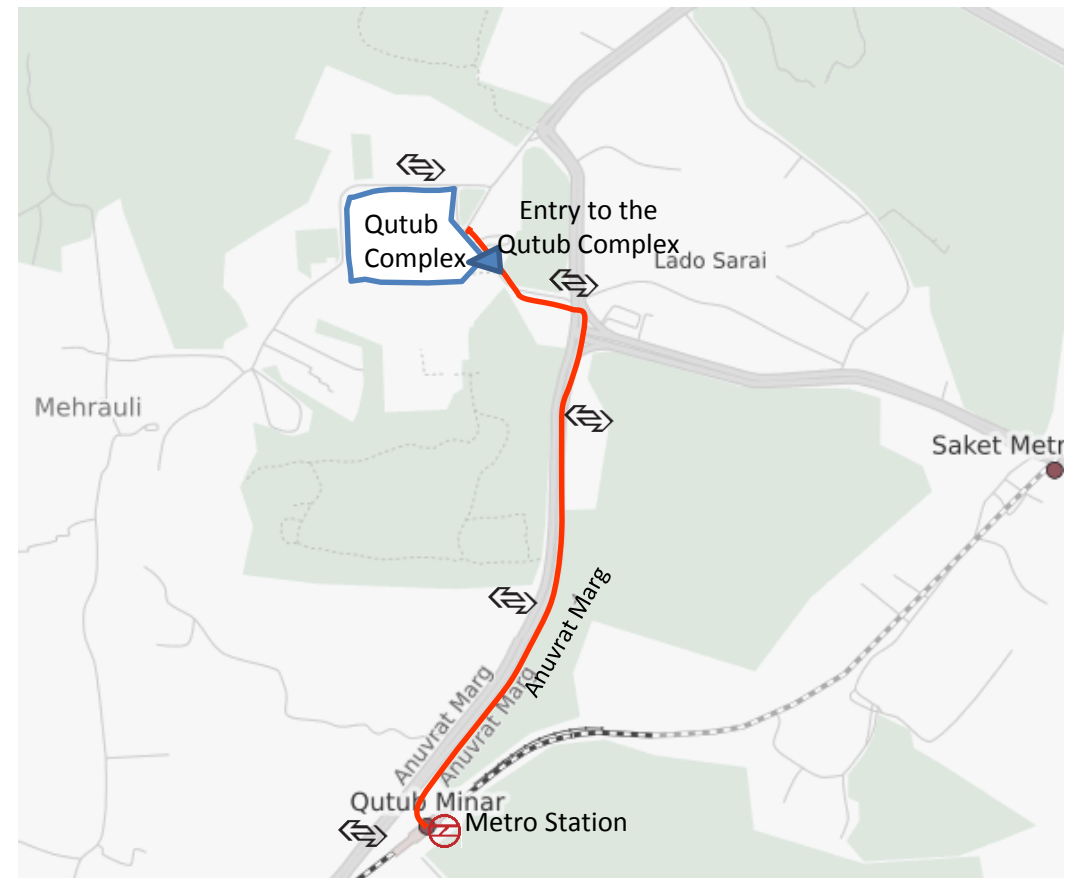


Qutub Minar

Safety Audit

The route from Qutub Minar metro station to Qutub Minar i.e. approximately 1.5 km has been audited and 100 audit pins have been generated. The nearest bus stop is located at a distance of 50 m from the tourist place. The area outside the tourist place entry/exit and the route to the metro station were studied.

Nearest Metro Station – Qutub Minar (Yellow Line)












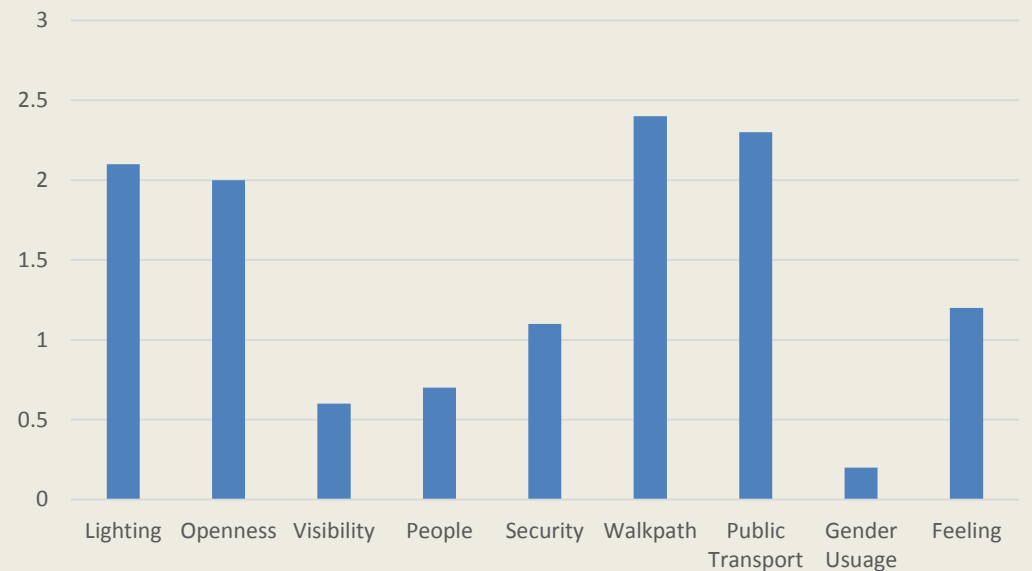
Metro Station

Bus Stop

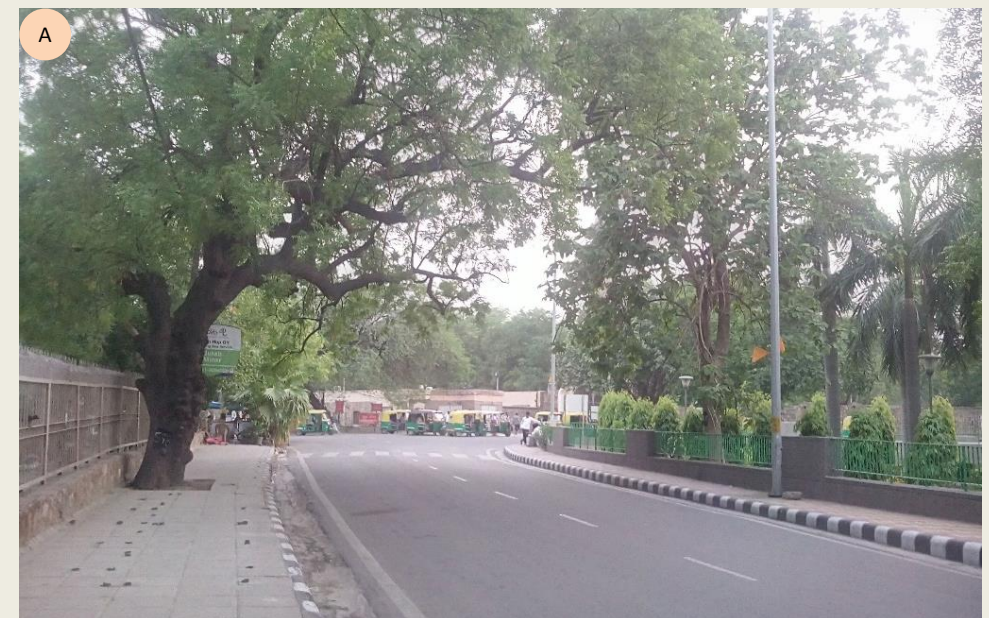
Location of the tourist spot
 Audit path followed

SAFETY AUDITS ANALYSIS

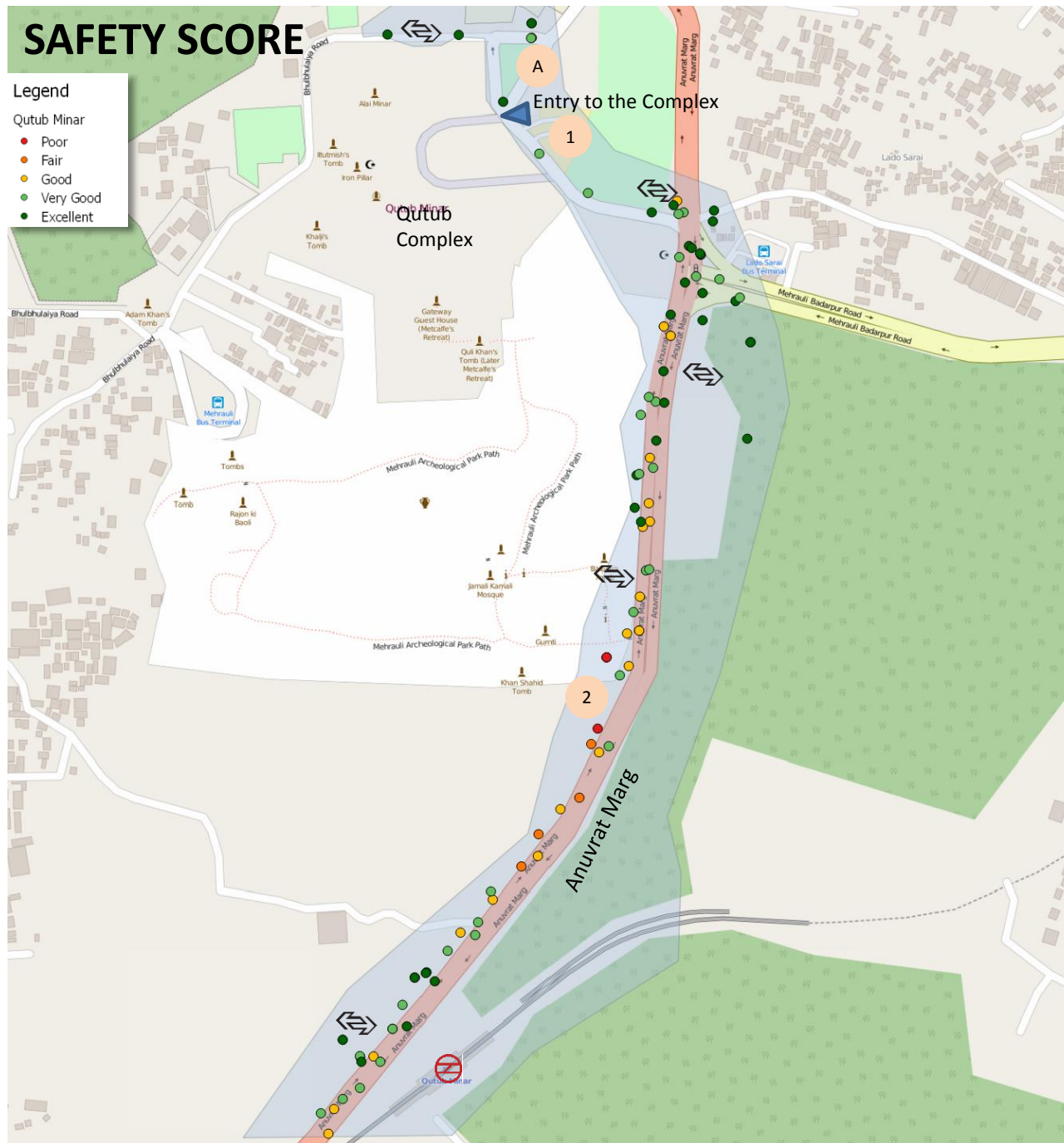
	2.1 Above Average Streetlights are present throughout the stretch and were found to be operational.
	2.0 Above Average It is a four lane wide road with clear line of sight.
	0.6 Poor Visibility is rated poor due to absence of hawker zone or market throughout the audit stretch
	0.7 Below Average People is rated below average since pedestrian count is low
	1.1 Below Average Security is rated below average due to absence of any police surveillance on the Anuvrat Marg at the time of audits
	2.4 Above Average Footpath is provided throughout the stretch but is broken or obstructed at few locations.
	2.3 Above Average Bus stops are present at regular distance and autos are easily available.
	0.1 Poor With low pedestrian count, few women and children were seen at the time of audits
	1.2 Below Average Feeling is rated below average owing to poor visibility and low security.



Average of all parameters (on scale of 3)



The path leading to Qutub Minar



Map showing Safety Score rating



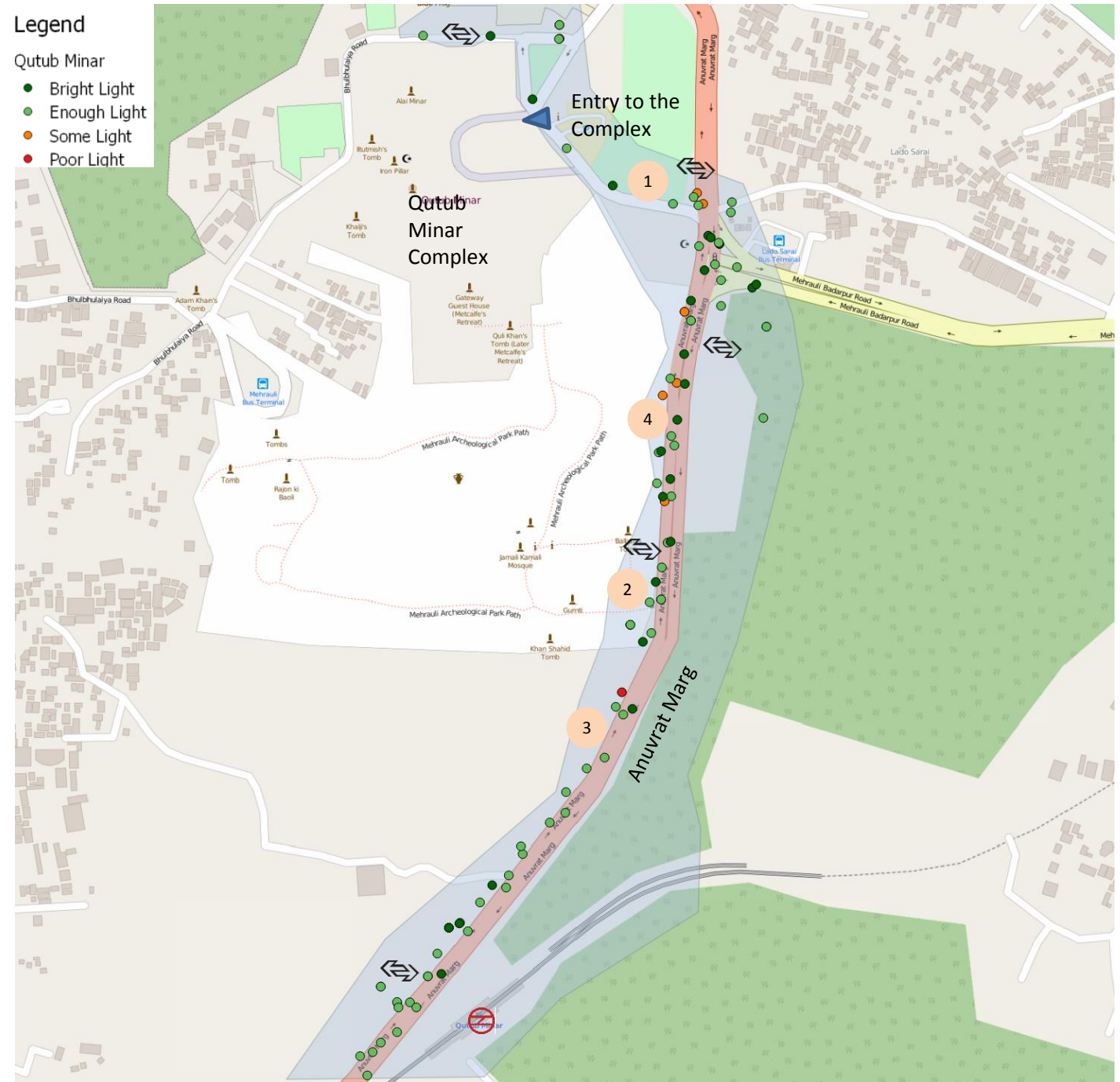
The entry/ exit to the Qutub Minar has high rating in Visibility, People, Security, Gender Usage and Feeling parameters.



This audit point is given low rating in Lighting, Visibility, People, Security, Gender Usage and Feeling parameters.

LIGHTING

Lighting is rated 2.1/3 i.e. Above Average. Streetlights exists on most of the stretch. On Anuvrat Marg, street lights are provided on the central median focussing on vehicular carriageway. There is absence of pedestrian scale lighting on the footpath. Streetlights installed along the stretch from Anuvrat Marg to Qutub Minar are hidden behind trees' leaves, resulting in dark spots.



Map showing lighting rating

1 On this stretch of road, streetlights are placed too far and some are hidden behind the trees' leaves. Additional light poles should be installed alternately on the other side of the road, that will ensure uniform illumination throughout the road. Also, the foliage along the streetlight needs to be pruned regularly.

2 Lights installed at the entry/exit of a complex were found to be non-operational as seen in front of Mehrauli Archaeological Park (image 2). Similar cases were found in front of Delhi Jal Board's STP and Central Bharat Sevak Samaj. These lights should be made operational so as to illuminate the entry/exit of a complex.

3 As seen in the images, the streetlights have been provided on the central median only thereby, focusing on the vehicular carriage way of the road. This results in poorly lit footpath. Pedestrian scale lighting should be provided along the boundary wall ensuring uniform illumination. Also, tree leaves should be regularly pruned.

4



WALK PATH

Walk path is rated 2.4 i.e. Above Average. Walk path exists throughout the audit stretch but was found to be broken and obstructed by construction debris, open manholes, signage boards etc. Improperly placed signage boards can prove to be risky for the visually challenged. They should be removed to make way for smooth pedestrian movement. Also, the walkpath should have tactile paving, accessible through a gradual ramp.



Map showing walkpath rating

- 1 The footpath is provided throughout the audit stretch but as seen in image 1 and 2, it is obstructed by street vendors, signage boards, pole stand etc. at various spots. These obstructions should be removed to make way for uninterrupted paved footpath. Footpath should be widened to create space along the built edge for hawkers.
- 2
- 3 A continuous paved footpath should be constructed to connect it with the new Public Convenience facility being set up, as seen in this image.
- 4 On Anuvrat road, footpath was found to be broken at several spots. As seen in the image 4, improperly covered manhole and pipe can be risky for the pedestrians. These should be covered and the cover should be flushed with the level of the footpath to ensure smooth movement of the pedestrians.



VISIBILITY

Visibility is rated 0.6 i.e. Poor. This stretch is characterized by continuous boundary walls of huge parks which houses various monuments of archaeological importance. Therefore, the audit stretch was largely found to be deserted with few hawkers in between.



Map showing visibility rating

1 As mentioned earlier, the audit stretch has many dead spots that results in poor visibility. However, few vendors and temporary stalls are present at some distance on the audit stretch. The spots seen in image 1, 2 and 3, can be turned into a hawker zone. At spot seen in image 1, there are few temporary stalls.

2 At spots seen in image 2 and 3, there is an existing flower mart and nursery respectively. With proper lighting and equipped with street furniture, these spots can be designated as a hawker zone, thereby activating the area.

4 Near the ticket counter, there is a hawker zone with temporary stalls, public convenience and street furniture. However, many street vendors occupy the area outside the entry/exit of Qutub Minar. To accommodate these vendors, another hawker zone can be set up near the entry/ exit of Qutub Minar. Also, a designated auto stand should be set up with proper parking space for the autos, clear of the footpath.



RAJGHAT

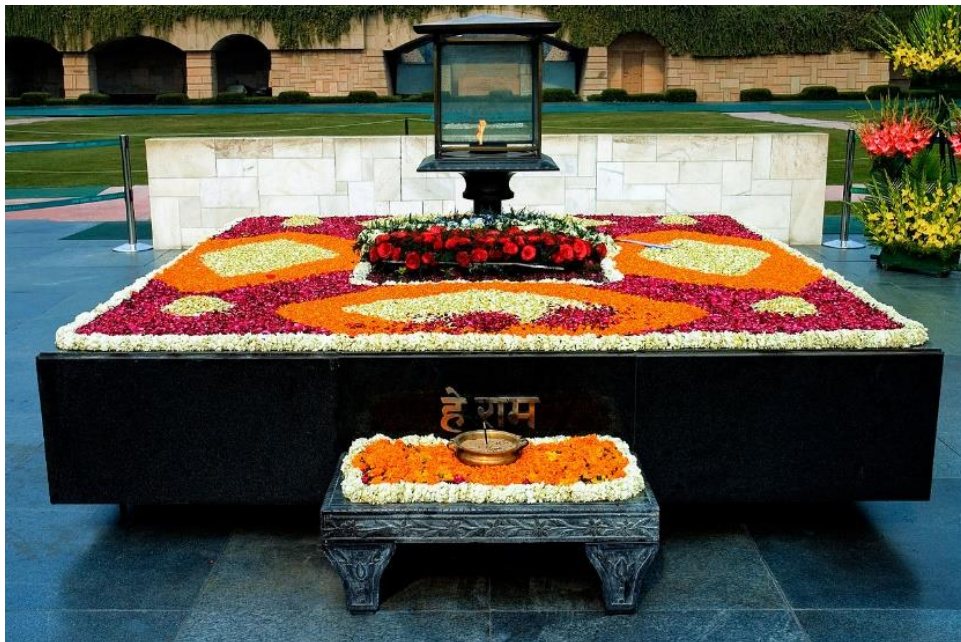
Safety Score: **3.3** / 5

About

Rajghat is located on the banks of the river Yamuna on Ring Road, officially known as Mahatma Gandhi Marg. Rajghat is a memorial of Father of Nation, Mahatma Gandhi. It is a black marble platform that marks the spot of Mahatma Gandhi's cremation. Several other *samādhi* or cremation spots of other famous leaders can be found in the vicinity of Rajghat.

Social Usage

Along with these memorials, this area has various government, media and publication offices on Bahadur Shah Zafar Marg. This area also sees protest marches and dharna from time to time in front of Delhi Government offices.

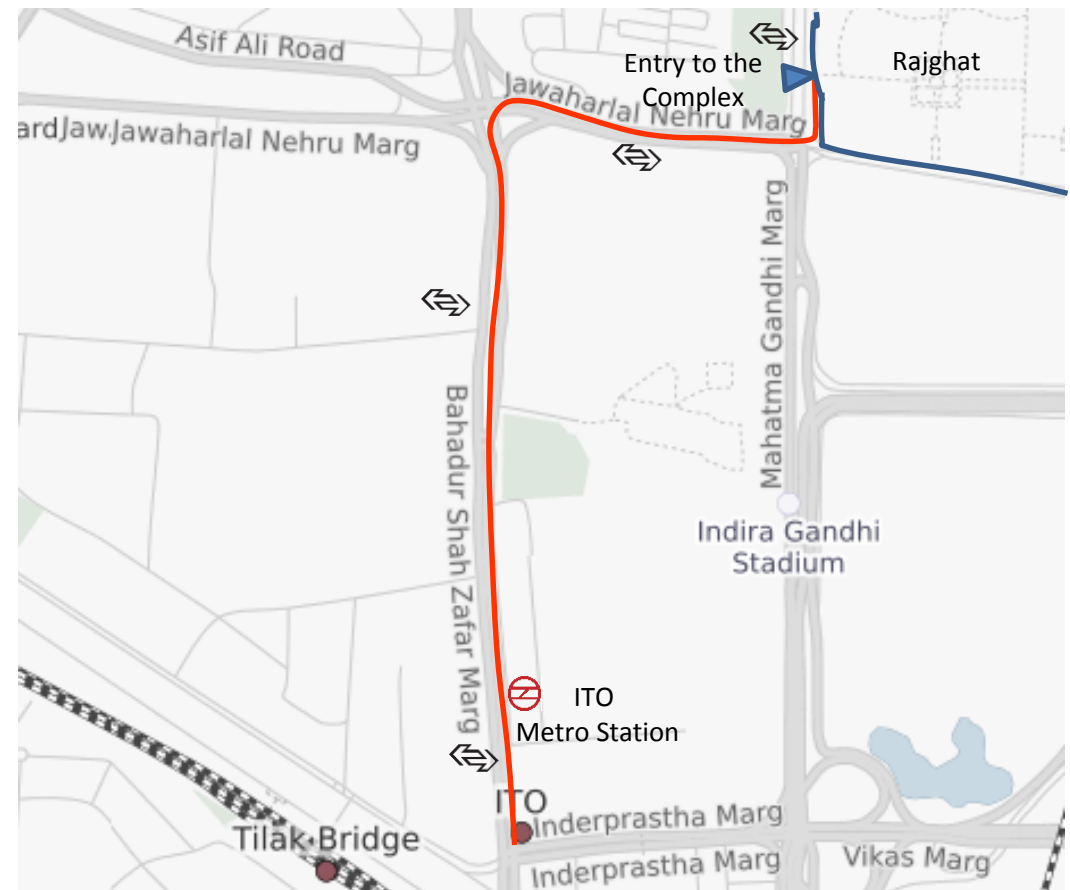


Rajghat

Safety Audit

The route from ITO metro station to Rajghat i.e. approximately 2.0 km has been audited and 40 audit pins have been generated. The nearest bus stop is 100 m away from the tourist place. The area outside the tourist place entry/exit and the route to the metro station were studied.

Nearest Metro Station – ITO (Violet Line)



Location of the tourist spot



Metro Station












Bus Stop



Audit path followed

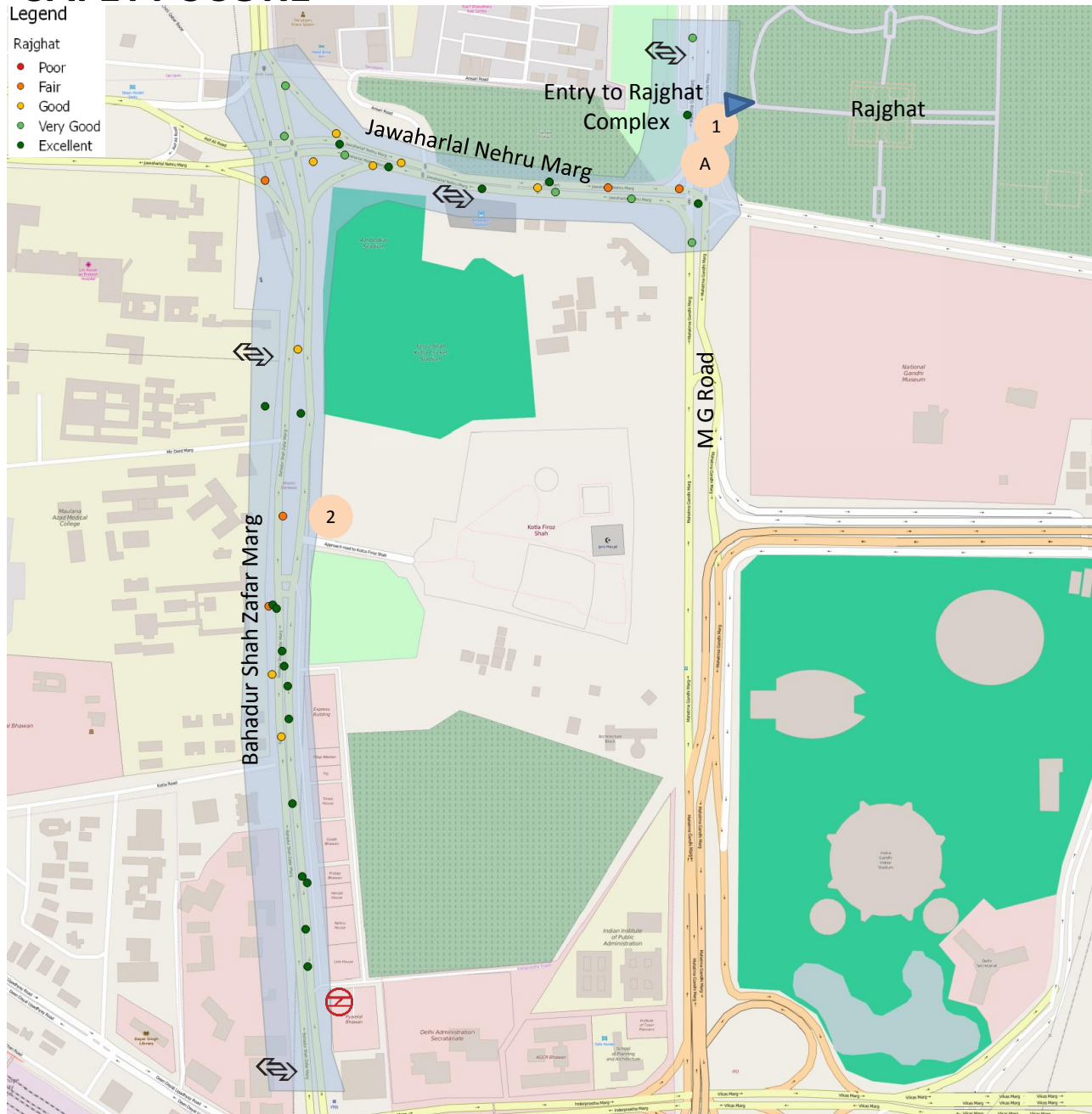
SAFETY AUDITS ANALYSIS

 Light (Night)	2.0 Above Average Adequate lighting is provided along most of the audit stretch.
 Openness	2.0 Above Average A pedestrian can see clearly the surroundings and nearby areas walking through the audit stretch.
 Visibility	1.2 Below Average Visibility is rated poor due to absence of hawker zone or market throughout the audit stretch
 People	0.9 Below Average People is rated below average since pedestrian count is low in evening.
 Security	1.5 Average Police patrolling Van is seen patrolling the area throughout a day.
 Walk Path	2.0 Above Average Footpath exists in most of the audit stretch but is non-continuous and not maintained properly.
 Public Transport	2.3 Above Average Bus Stops are present at regular distance and autos are easily available in this audit stretch.
 Gender Usage	0.7 Below Average Gender Usage is rated poor as very few women and children were seen at the time of audits
 Feeling	1.3 Average Overall, feeling is rated average by the auditors.



The path leading to the Rajghat

SAFETY SCORE



Map showing Safety Score rating



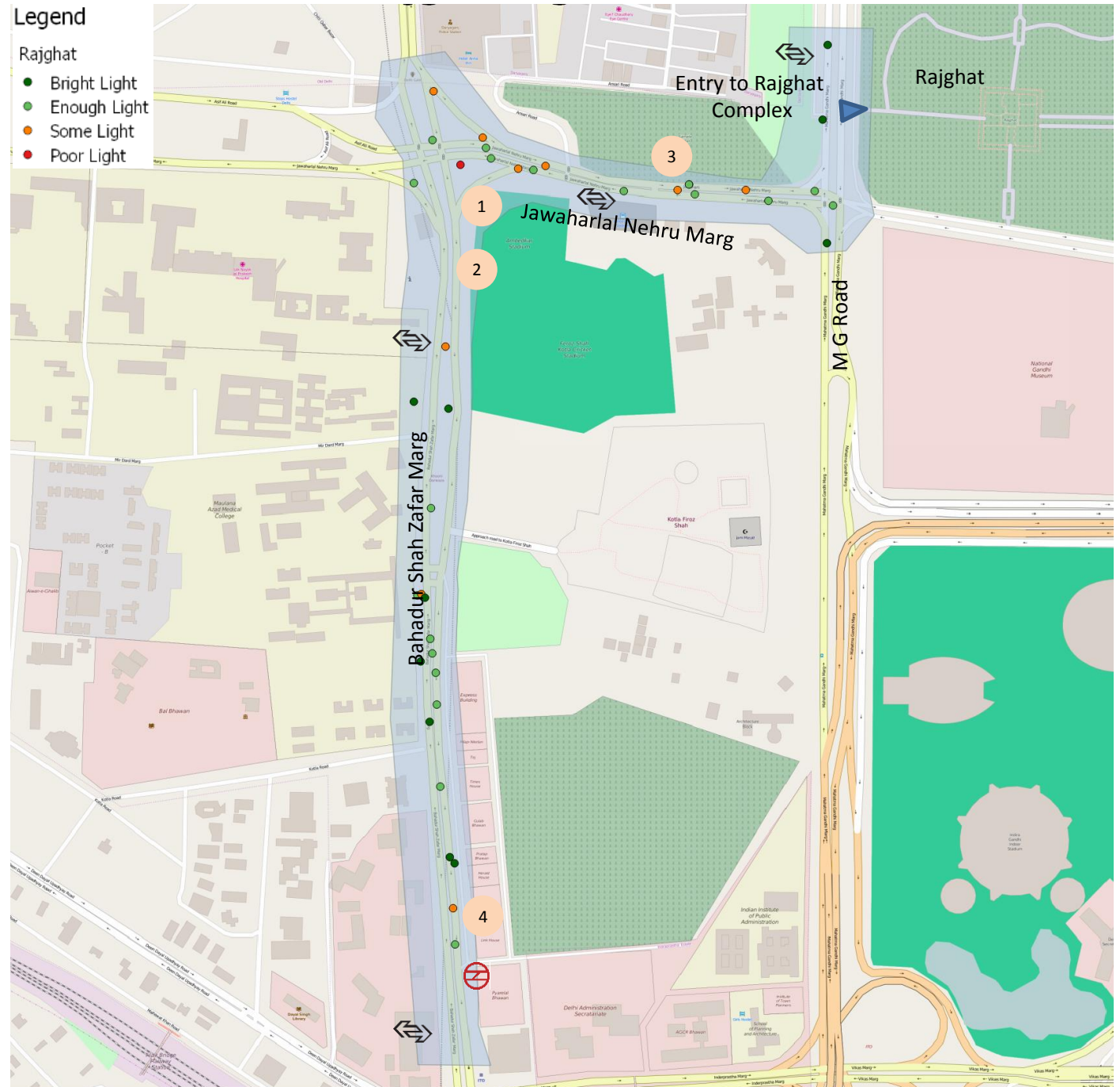
The entry/exit to Rajghat has been given high rating in Lighting, Openness, Security, Walk Path, Public Transport and Feeling.



The audit point has been given low rating in Lighting, Visibility, Security and Feeling.

LIGHTING

Lighting is rated 2.0/3 i.e. Above Average. Streetlights are installed throughout the stretch. However, some of them are not functional, thus creating dark spots in between. On Bahadur Shah Zafar Marg, there are pedestrian scale lighting installed along the footpath. This needs to be done on Jawaharlal Nehru Marg as well.

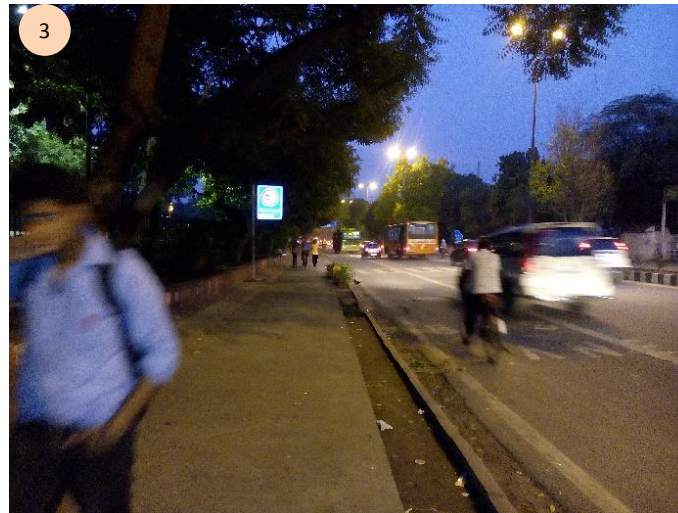
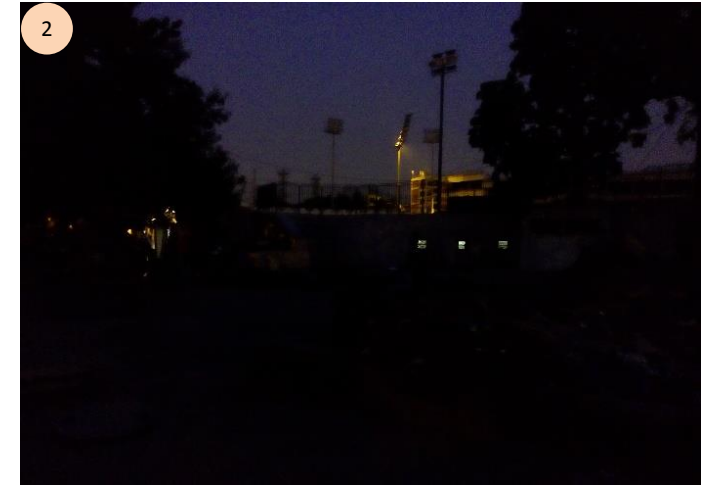


Map showing lighting rating

1 Though streetlights are present throughout the stretch, some of them were found to be non- operational. The stretch near Firoz Shah Kotla Stadium to Kotla Firoz Shah has many dark spots due to non – operational streetlights as seen in image 1 and 2. These streetlights should be checked and made operational.

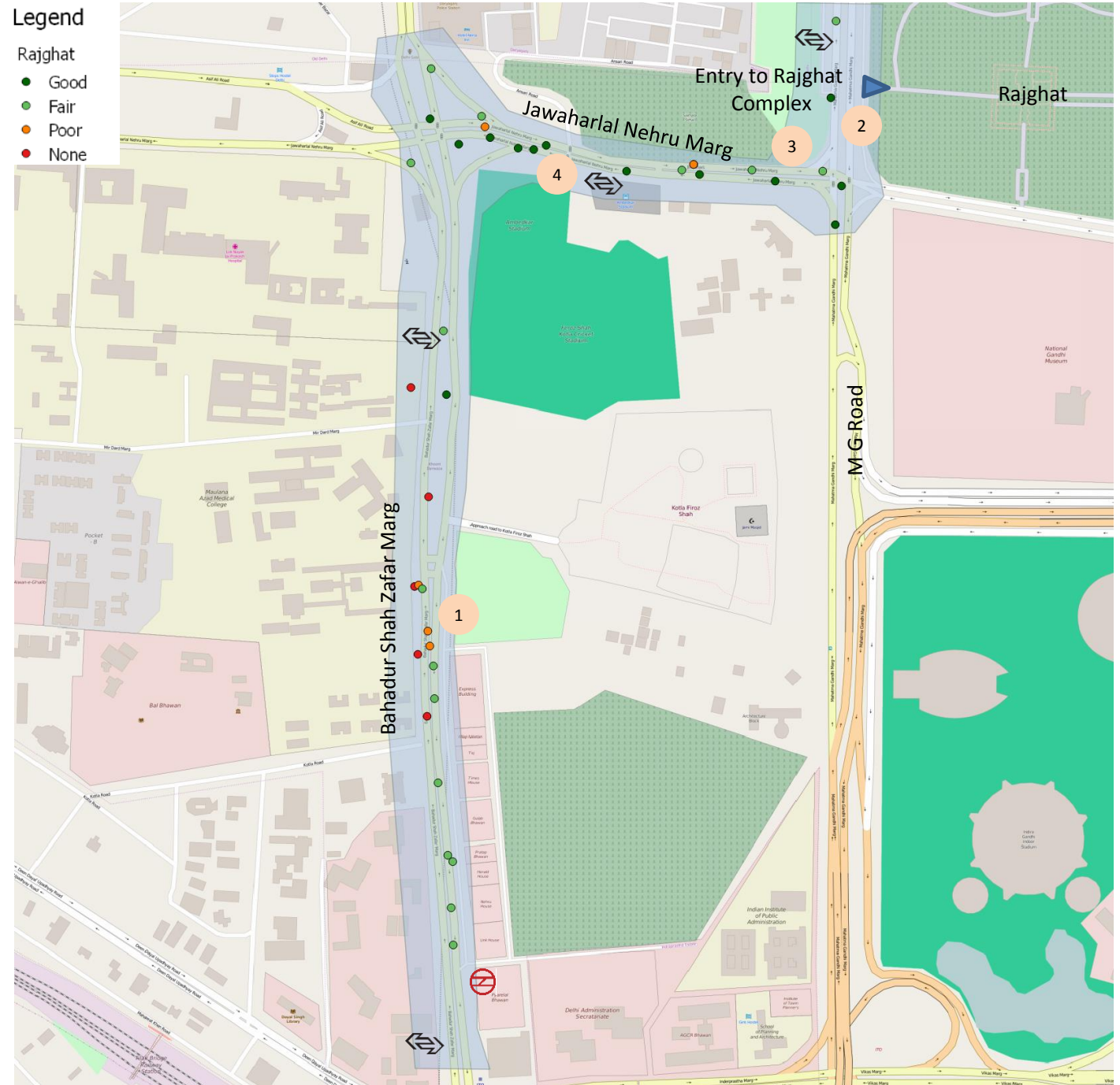
3 As seen in the image, the streetlights have been provided on the central median only thereby, resulting in poorly lit footpath. Pedestrian scale lighting should be provided along the grills of the planters ensuring uniform illumination.

4 At present, the service road is lit owing to building's lights. The foliage along the streetlight as seen in image 4, needs to be pruned regularly to provide uniform illumination.



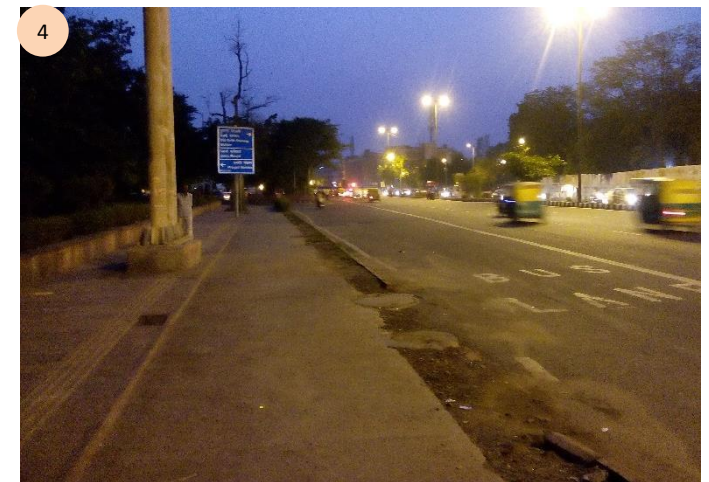
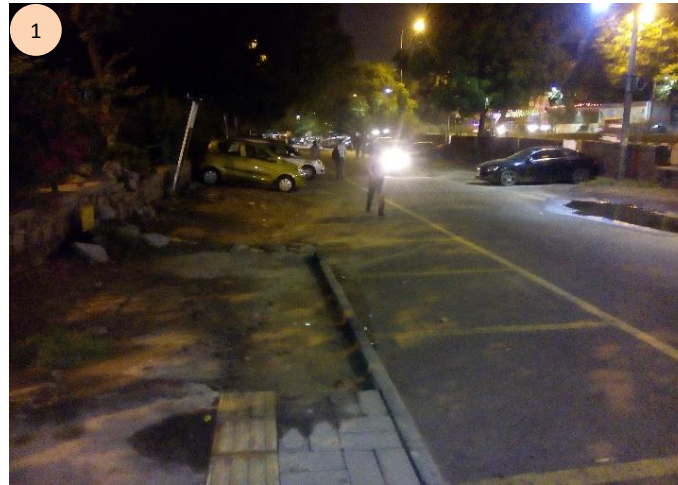
WALK PATH

Walk path is rated 2.0/3 i.e. Above Average. Footpath exists throughout the audit stretch but was found to be broken and non continuous. Obstructions like construction debris, unflushed manhole covers etc. can prove risky for the pedestrians. These need to be removed to make way for uninterrupted paved footpath.



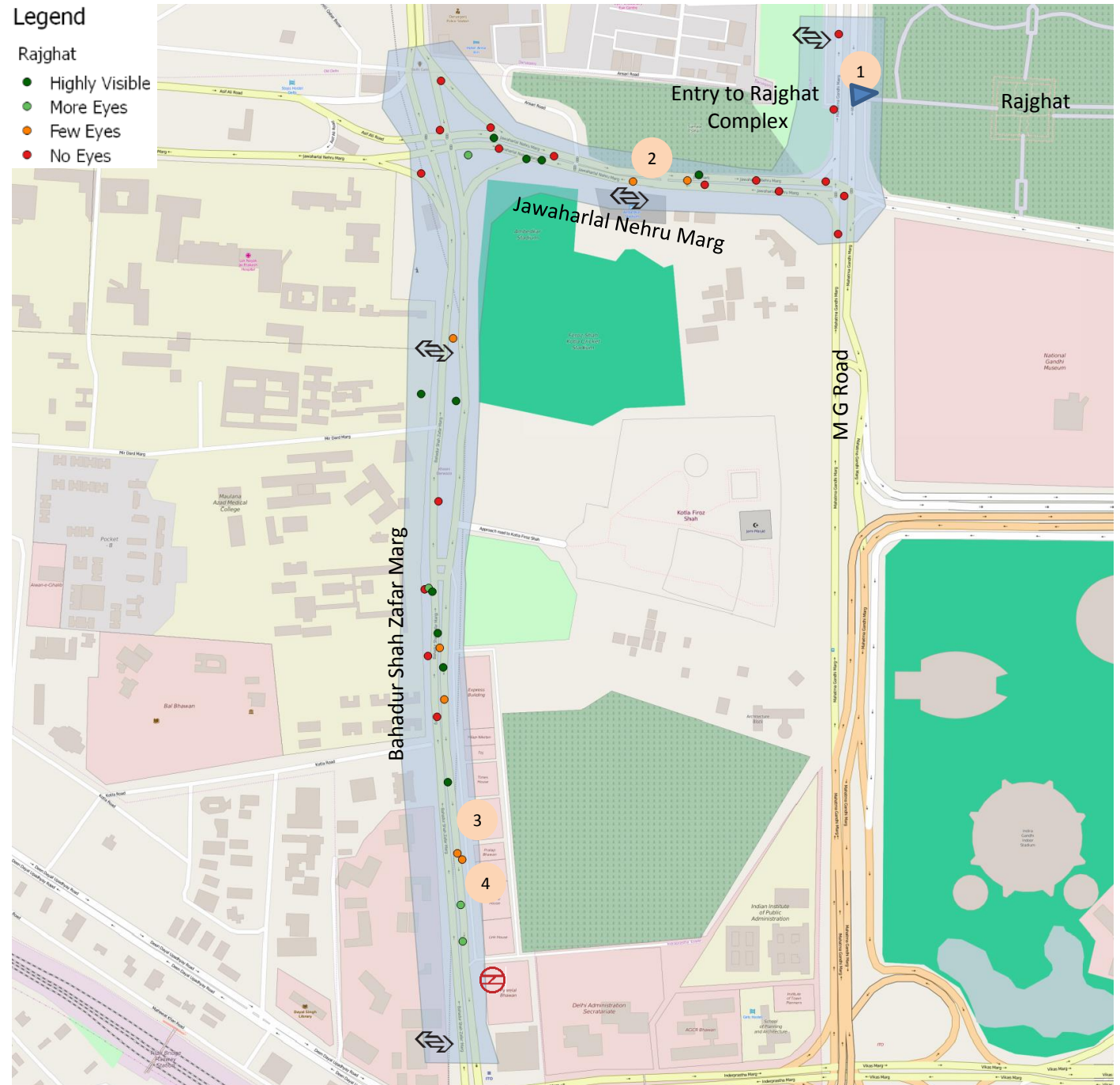
Map showing walkpath rating

- 1 At some spots, the footpath is broken and non- continuous as seen in image 1.
- 2 The tactile paving ends abruptly which can prove extremely risky for visually challenged people. In image 2, the tactile paving is placed in non- linear pattern and too close to the planter, that can prove inconvenient for the visually challenged people. A continuous paved footpath with tactile paving should be constructed, clear of any obstructions. The footpath should be widened wherever it is obstructed by a tree. In case of image 2, the planter should be restricted to one side thereby providing a linear footpath.
- 3 Seen in image 3, the broken footpath with unflushed manhole cover can prove risky for the pedestrians. This needs to be repaired and the cover should be flushed with the level of the footpath.
- 4 On Jawaharlal Nehru Marg, the tactile paving on a wide footpath is obstructed by the signage board. This can prove risky for the visually challenged. This tactile paving should be removed, and a new one should be constructed, clear of any obstruction.



VISIBILITY

Visibility is rated 1.2/3 i.e. Below Average. There are number of street vendors present on the audit stretch but in absence of a designated space, they occupy the footpath or service road. A designated space should be provided for the street vendors as they offer much needed “eyes on the street”.

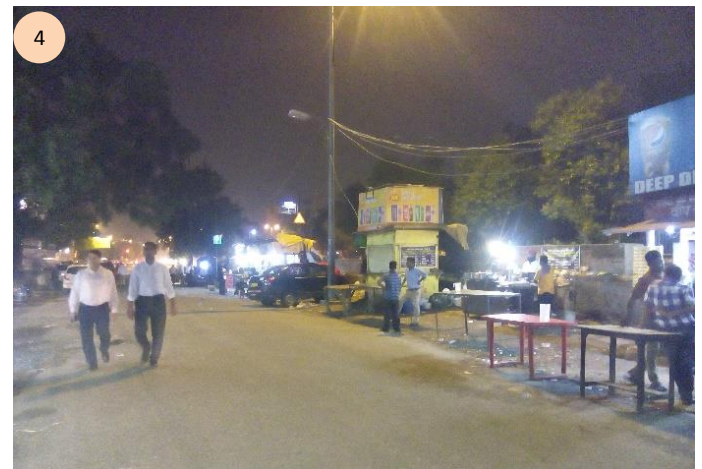


Map showing visibility rating

1 Few street vendors can be seen in image 1 i.e. in front of Rajghat Complex. Autos are present too. This space can be further enhanced by making it a para transit hub. A designated auto stand should be set up with proper parking space for the autos, clear of the footpath. Also, a hawkker zone should be created, equipped with street furniture for people to sit and wait.

2 On Jawaharlal Nehru Marg, few vendors can be seen near the bus stop. The bus stop on one side of this road doesn't have a proper shelter for people. This space should be redesigned to create a bus shelter, and a designated space for these street vendors.

3 The service road in front of Times of India and Indian Express office, as seen in image 3 and 4 has many temporary stalls and street vendors, catering to the employees. This stretch should be redesigned to provide designated space for these stalls and vendors, equipped with street furniture.





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