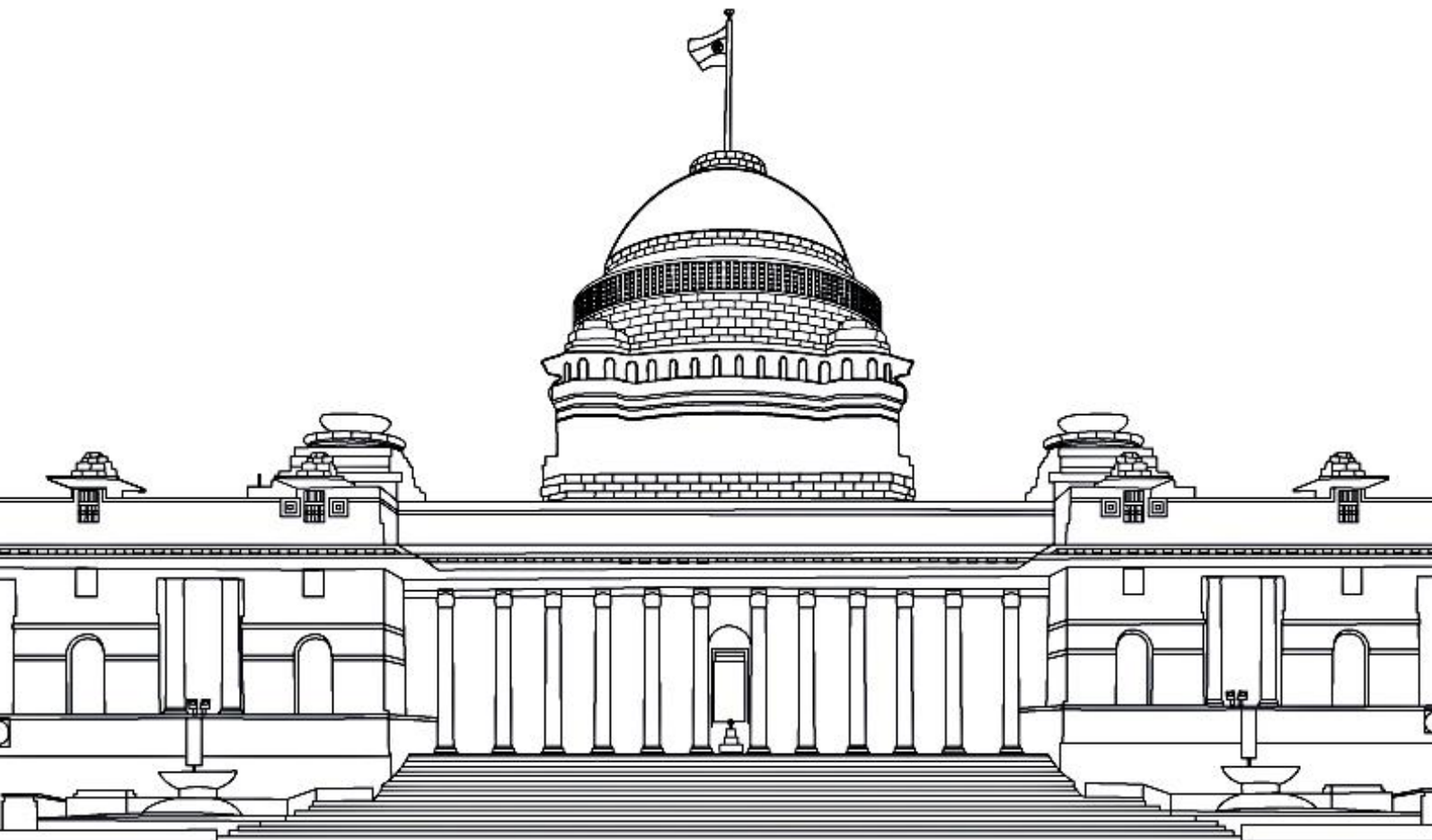


ENHANCING LAST MILE CONNECTIVITY

a safety analysis of the Central Secretariat Metro Station



This Report has been prepared as part of the Project being undertaken with NDMC to Enhance the Last Mile Connectivity along the metro stations within it's jurisdiction. The safety audits were conducted by Smt. Jaspal Kaur and Smt. Manju, architects from NDMC along with Safetipin team.



Central Secretariat

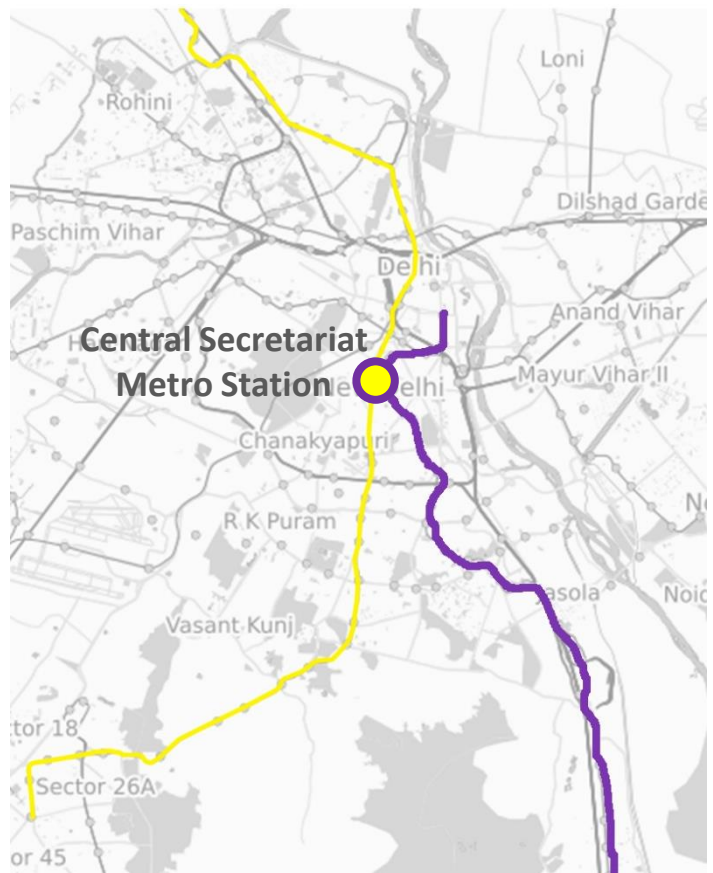
SAFETY SCORE: 3.9/5

SafetiPin, is a map-based mobile phone and online application, which works to make communities and cities safer by providing safety-related information collected by users and by trained auditors. At the core of the app is the Women's Safety Audit. A Women's Safety Audit (WSA) is a participatory tool for collecting and assessing information about perceptions of urban safety in public spaces. The audit is based on nine parameters – Lighting, Openness, Visibility, Crowd, Security, Walkpath, Availability of Public Transport, Gender Diversity and Feeling.

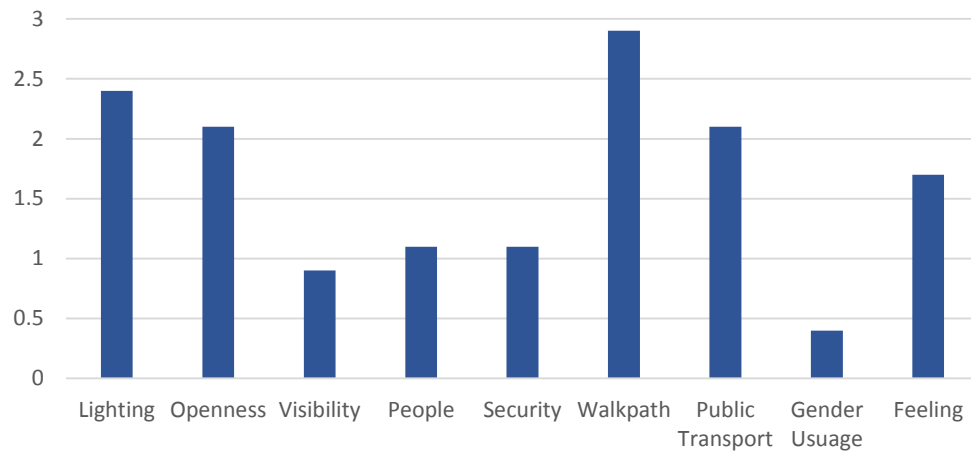
The audits were conducted by architects from NDMC along with Safetipin team. The assessment was done post sunset till 9pm.

Central Secretariat is an underground metro station that provides a same-level interchange between the two lines of Delhi Metro namely Yellow line and Violet line. Located in the administrative zone of the city, this station experiences high footfall daily.

An area of approximately 500m radius around the metro station has been studied and 67 audit pins have been generated. The area outside the metro entry/exit and the Bus Stop were studied. The areas around the Government's various government offices were studied in detail.



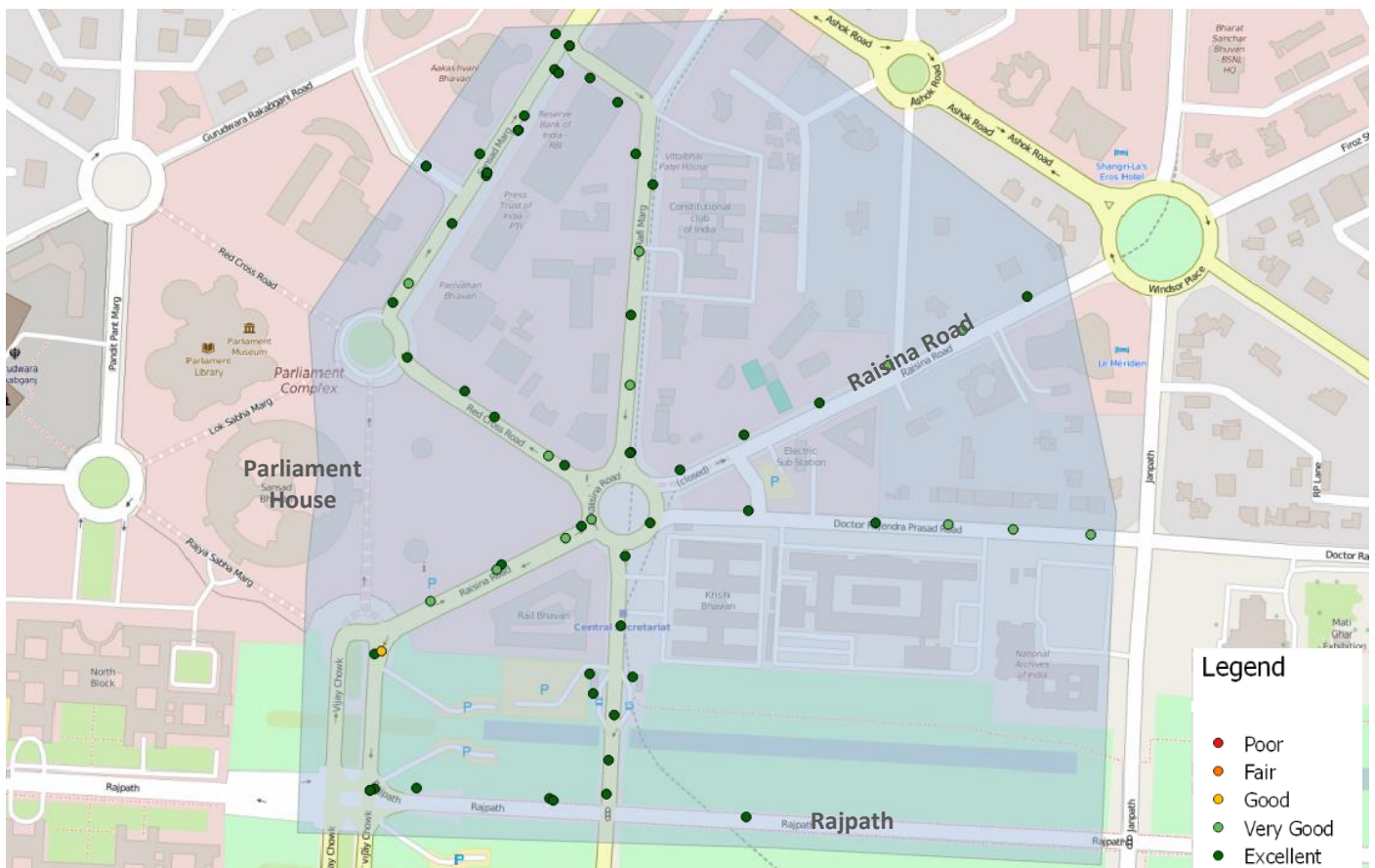
Average Audit Parameters (on a scale of 3)



Safety Audits indicate that the area around the metro station is safe. The parameter of Lighting has been rated Above Average. Being a part of Lutyens' Delhi, this area is well designed and managed. The footpath is present throughout the audit area and is rated Good.

Owing to the availability of interchange stops for metro and bus, the parameter of Public Transport has been rated Above Average.

The parameters of Visibility and Security have been rated Below Average. Being in an administrative zone, the area has continuous Police Patrolling. However, the huge setbacks and high boundary walls offers no visibility on the walkpath. Also, the presence of people and especially women on the streets after office hours is extremely low. Overall, auditors have rated the Feeling in the area as Average.



Map indicating Safety Score

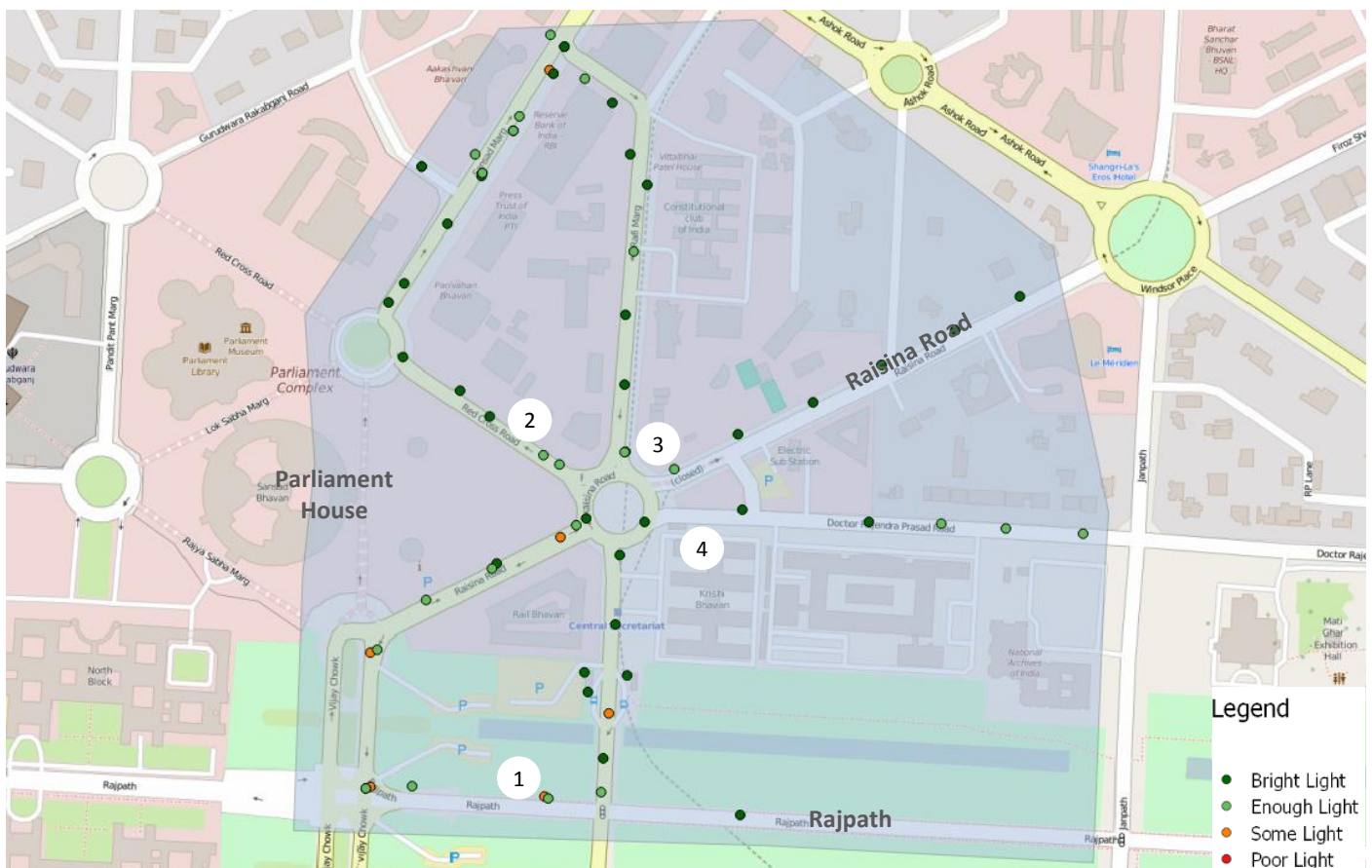
Lighting

Lighting Parameter has been rated 2.4/3 i.e. Above Average. Overall, lighting was found to be enough for clear vision. However in some areas, the streetlights provided focus towards the road resulting in poorly lit footpath.

In an administrative zone, where pedestrian movement is more, the streetlights are provided facing the roads. This results in well lit road but dim lit footpath.

On Rajpath, streetlights are placed on the edge of the footpath, thereby illuminating walkpath as well as the road.

All the radial junctions are brightly lit, however along the inner roads within this area, some streetlights are hidden behind foliage resulting in low levels of lighting in the area.



Lighting Rating



Most of the area around the Central Secretariat Metro Station is well lit. However, some streetlights were found to be non-operational on Rajpath leading to the Rastrapati Bhawan. As a result, the lighting along this path is low. Streetlights need to be made operational so that the area is well lit.



Currently, the streetlights have been provided focusing on the vehicular carriage way of the road. Seen here in Pic 2, streetlights are focused on carriageway resulting in poorly lit footpath. Additional lights should be installed on the edge of the footpath or along the boundary wall, facing the footpath.



Dark spots are created owing to absence of light on the footpath. Installing pedestrian scale light poles along the walkpath will ensure uniform illumination.

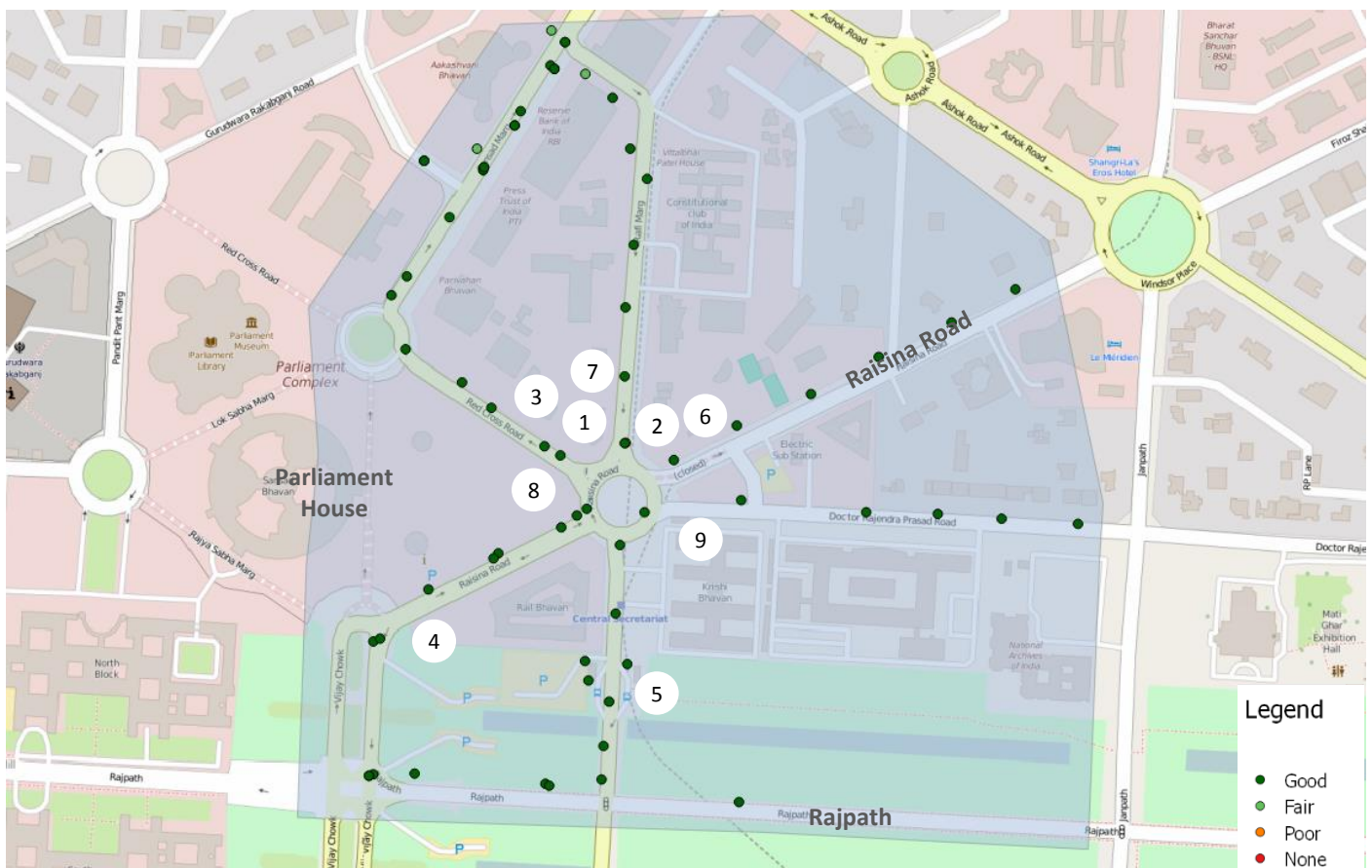


Also, the foliage along the streetlights needs to be pruned regularly as the overgrown leaves shield the light fixture resulting in poor illumination

Walkpath

Walkpath Parameter has been rated 2.9/3 i.e. Good. The footpath exists throughout the audit area and is well maintained. However, commonly observed on footpath are various types of obstructions— planters, signage, advertisement boards etc.

In an administrative zone with high footfall, there are street vendors obstructing the walkpath. Some street vendors are provided a designated kiosk. Similar provisions should be provided for other hawkers so that the pavement is clear for pedestrian use.



Walkpath Rating



Though the footpath exists in good condition throughout the audit path, they are obstructed at some points. As seen in the Pic 1, planters and advertisement board in middle of the footpath can prove nuisance for pedestrians. These obstructions should be removed and placed at the edge of the footpath to provide a clear passage to ensure smooth movement of the pedestrians.



Seen here in Pic 2, a man in wheelchair is using main road in midst of oncoming vehicles. Also seen in Pic 3, the pedestrian access to the building isn't flushed with the footpath outside the building through ramp, which makes commuting difficult for disabled people. Also, improper slope (Pic 4) can prove risky for them. A continuous paved pathway should be provided which is accessible through ramp connecting them to the pedestrian entry/exit of a building.



Tactile paving outside the metro station ends abruptly, making it difficult for the visually impaired person to walk alone. As a result, they always need assistance right from the metro entry/exit to the bus stop/ surrounding government's offices. In order to make Public Transport easily accessible, proper tactile paving should be provided from the metro station to the nearest bus stop (Pic 5), clear of any obstruction.

The street vendors should be located in a way that doesn't obstruct the pedestrian movement. As seen in Pic 6, there is a wide footpath that experience high footfall, a hawker zone should be set up clear of the footpath.



Seen here in Pic 7 are vendors provided with designated kiosks. In addition to that, proper lighting on walkpath and street furniture should also be provided for the people.



Footpath should be continuous and clear of any obstruction. As seen in the Pic 8, footpath is obstructed by a tree forcing people to walk on unpaved buffer area. This buffer area should be properly paved, thus widening the footpath to ensure smooth movement of the pedestrians.



This footpath as seen in Pic 9 is flanked by shrubs and trees from both the sides, thus making it shady and unsafe at night. In such cases, height of shrubs should be maintained less than 1m, thereby providing a clear sightline to the pedestrians.

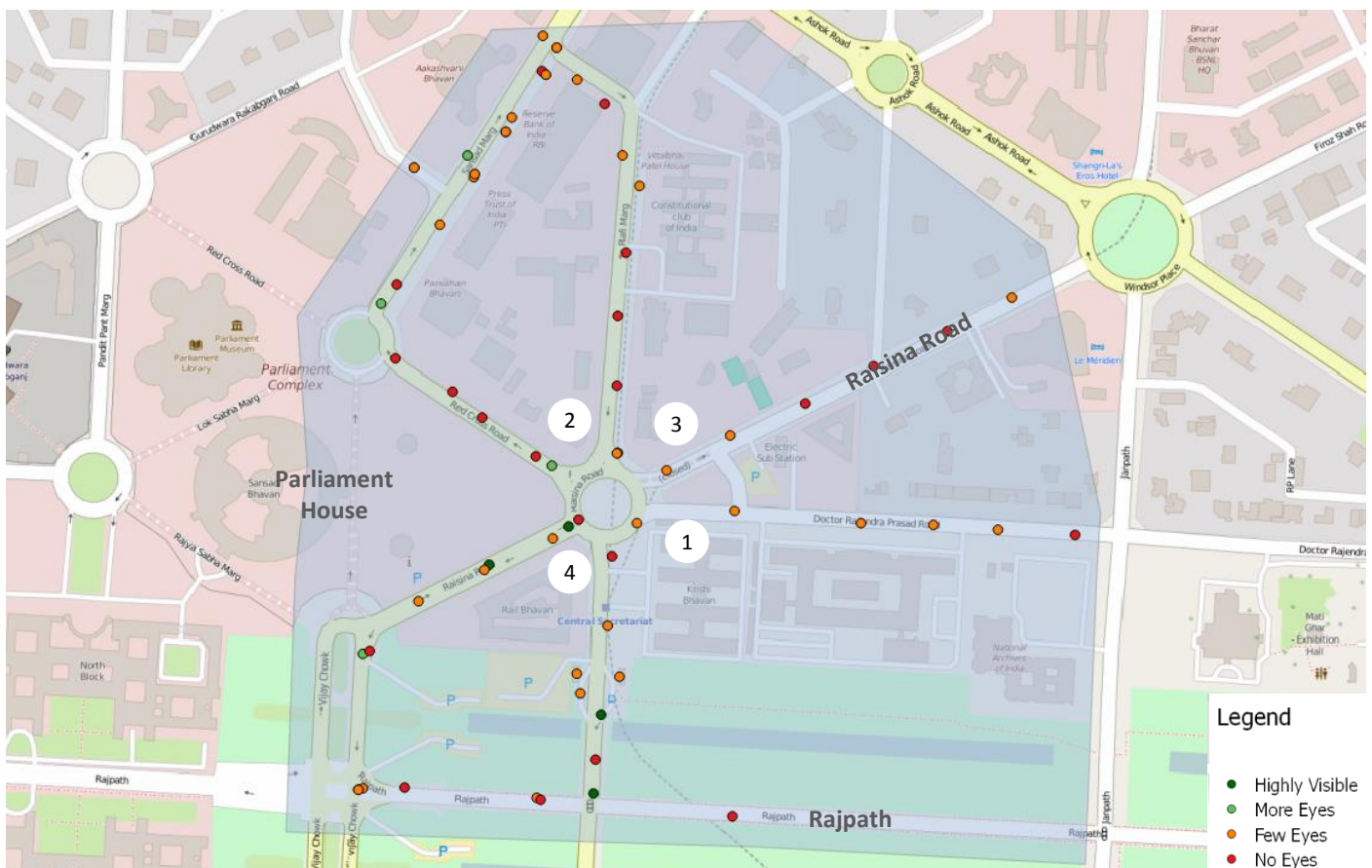


Visibility

Visibility Parameter has been rated 0.9/3 i.e. Below Average. With offices closed at 6 PM, there is hardly any activity on streets at night resulting in “no eyes on the street”. While there is some visibility near the entry/ exit of metro station due to the presence of vendors, largely the stretch offers very poor or no eyes on the street owing to high boundary walls. In such cases, the visibility can be improved by lowering the height of opaque boundary walls

These should be replaced with grills such that a visual connection is maintained between the pedestrian and the building’s premises. This would offer some security and eyes to the pedestrian on the street.

Visibility needs to be further enhanced by creating a designated space for the vendors in a way that, they do not obstruct the path of the pedestrians.



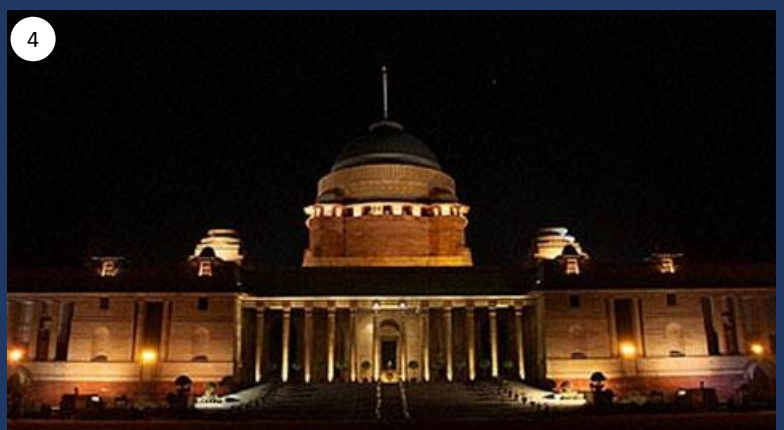
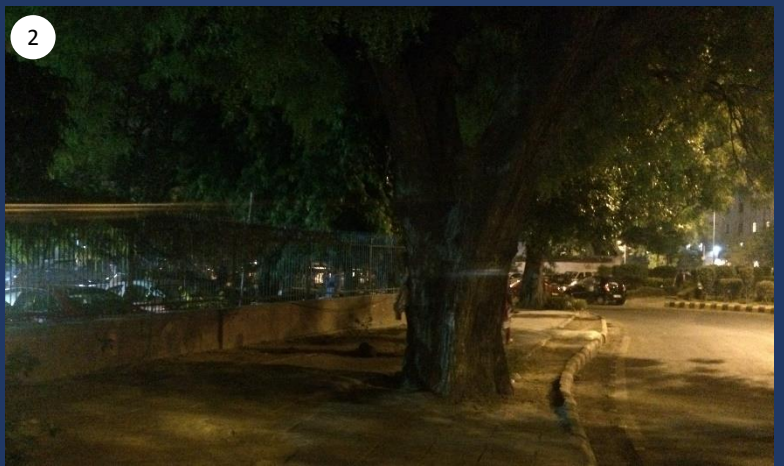
Visibility Rating

Seen here in Pic 1, the footpath is flanked by trees from both sides, making it unsafe at night. The boundary wall design of these areas should be modified. The solid surface of the boundary wall needs to be reduced to atleast 1m and the remaining height be achieved using a grill. (as seen in Pic 2)

Dead spots are created due to absence of activity and light. To activate this stretch, the space should be redesigned in a way that a shaded space equipped with street furniture is provided for the pedestrians, which can also accommodate the street vendors.

In absence of a designated space, hawkers end up crowding the footpath (Pic 2). At places where footpath is wide enough, a hawker zone should be set up clear of the footpath. Also, lack of street furniture forces people to squat on the pavement. Street furniture should be provided for them.

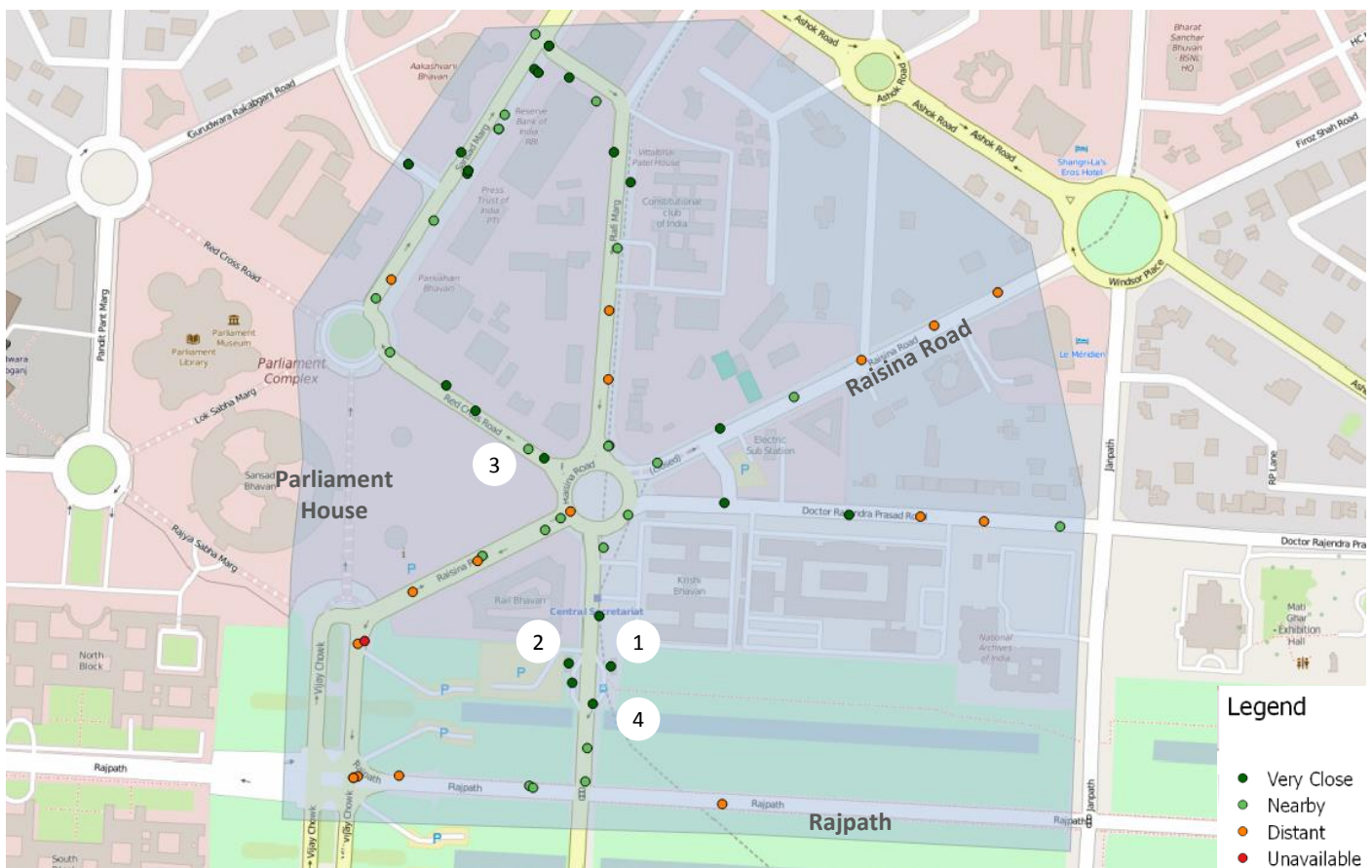
The area has government offices which all close in the evening. The area becomes extremely secluded in the post-office hours. Since this area forms a part of our Built Heritage, promoting Tourism through Heritage Walks and Night Photo-walks can help bring in more people in the area.



Public Transport

Public Transport Parameter has been rated 2.1/3 i.e. Above Average. Being an administrative zone, this area is well connected through metro and bus. Autos are available at every entry/ exit gate of the metro station. However, proper stand is not provided for the autos. It results in autos occupying the main road.

Designated Para-Transit hubs for autos need to be created near the metro station and bus stop. These hubs should also be integrated with the existing temporary stalls and vendors. These hubs should be well lit and equipped with street furniture as well as public convenience for the people.



Rating of Public Transport

With the large number of people visiting India Gate and Rashtrapati Bhawan, autos are parked at every entry/exit of Central Secretariat Metro Station.

However, in absence of proper parking space, autos occupy main carriageway. This can be clearly seen in Pic 2, near Metro Station Gate no.3, there is a separate lane for the bus stop but no proper auto stand. This results in autos occupying both the main and bus lane.

The area around the metro station and the bus stop is inhabited by hawkers. As seen in Pic 3, outside the entry/exit of the metro station has many hawkers occupying an edge of the large pavement. This area should be redesigned to have segregated space for the hawkers equipped with the street furniture for people, clear of the footpath. Also, an Auto Stand should be provided along the bus stop clear of the footpath.

Seen here in Pic 4 at the bus stop, footpath is obstructed by inappropriately placed advertisement board, dustbins and signage. While designing a bus stop, street furniture should be provided clear of the footpath.

